

**WEST HAYMARKET JOINT PUBLIC AGENCY (JPA)**  
**Board Meeting**  
**August 17, 2012**

Meeting Began At: 3:00 P.M.

Meeting Ended At: 3:22 P.M.

Members Present: Chris Beutler, Eugene Carroll, Tim Clare

**1. Item 1 -- Introductions and Notice of Open Meetings Law Posted by Door**

Chair Beutler opened the meeting with introductions of the Board members. He advised that the open meetings law posted at the back of the room is in effect.

**2. Item 2 -- Public Comment and Time Limit Notification**

Chair Beutler welcomed public comment. He stated that individuals from the audience will be given a total of five minutes to speak on specific items listed on today's agenda. Those testifying should identify themselves for the official record and sign in.

**3. Item 3 -- Approval of the minutes from the JPA meeting held August 3, 2012**

Beutler asked if there were any corrections or changes to the minutes of the August 3, 2012 meeting. Being none, Carroll moved approval of the minutes. Clare seconded the motion. Motion carried 3-0.

**4. West Haymarket Progress Report**

Paula Yancey, PC Sports, distributed the monthly information packet (attached hereto) on the status of the Pinnacle Bank Arena and associated infrastructure projects.

Yancey reviewed the participation numbers, indicating that through June/July of 2012 there had been 1,448 workers on site. This includes both professional and construction workers. Of the total, 59% (or 845) live in Lincoln/Lancaster County and another 27% (or 397) live in Nebraska outside Lancaster County. Only 14% (or 206) were outside Nebraska.

Looking at just the construction employees using certified payrolls or Davis Bacon, there were 1,109 workers on site. Of the total 665 (or 60%) of the workers are from Lincoln/Lancaster County, 326 (or 29%) from Nebraska, and only 118 (or 11%) outside of Nebraska.

For company participation, the total firms should be 234. Out of the 234 firms, 178 (or 76%) were Nebraska firms and only 56 (or 24%) were outside Nebraska. That equates to \$136 million out of a total \$184 million that was awarded to Nebraska firms that actually have an address in Nebraska.

One of the major milestones reached since the last update was the removal of all the railroad tracks by BNSF earlier than anticipated on the east, west, and north side of the project. This allowed Hawkins to mobilize for the core area roadway work, as well as Mortenson to expand their work opportunities under the east and west sides of the arena. We commend the hard work of BNSF, and Mike Schaefer in the Engineering Department in particular, in working with the team to facilitate removal of the tracks. The ‘X Yard’ track removal allowed Mortenson to start installation of piles. The ties were stockpiled, as one of the images shows, before BNSF loaded the ties onto railroad cars for proper disposal.

Another major infrastructure project continuing to be worked on is the remainder of the ‘N’ Street Project between 7<sup>th</sup> and 9<sup>th</sup> Streets. Constructors has been working diligently to finalize this project, which also included various pavement track removals for the RTSD. A picture is included showing this previous Monday, August 13, when one lane of traffic was opened in both directions in front of Baker Hardware. Another image shows addition lanes open heading towards Midwest Steel.

The packet includes an image showing Constructor’s paving the intersection of 7<sup>th</sup> and ‘N’ Street. They expect to have all lanes of traffic open to the public tomorrow morning and then the project will be substantially complete.

Hawkins Construction mobilized on the site. One of the first things was the operations on the Watson Brickson site in preparation for a temporary service parking lot. Environmental testing is still ongoing on this site as well.

Under the Hawkins core area roadway projects many utilities, including new water and sanitary sewer lines, will need to be constructed prior to the streets being paved. An image shows the tapping of a sanitary manhole along Pinnacle Arena Drive. A second image shows construction of a sewer pipe junction along the same drive. In the background, Mortenson is constructing the exterior architectural precast and prepping for the garage construction. The storm sewer pipe is shown laid in the area where the tracks were previously removed.

The historical canopy renovation is in progress. Hawkins and their subcontractor, Bockmann, have begun removing the historic canopy to prep it for the renovation. The image shows the old wooden roof panels being removed. Responding to Clare’s inquiry, Yancey confirmed the roof panels will be rebuilt with new wood since the old is too rotten to be reused during the renovation. A section of the canopy was taken down at ‘Q’ Street to allow the utilities to move through the area. The train engine was moved back as well to accommodate that work.

At the July 6 JPA Board Meeting, the Judds Brothers contract was approved to allow removal of the ‘Wye’ tracks, which were the only tracks BNSF elected not to remove themselves. The contract allowed 35 calendar days to complete removal after receiving notice to proceed. The notice was given July 10. Judds finished the work August 3, 11 days ahead of schedule and \$20,397 under budget.

John Hinshaw, Mortenson Construction, reported on the Pinnacle Bank Arena progress. He reported they are on time and on budget. The aerial view as of August 15 shows the first half of the roof truss going in on the southeast corner of the arena. The second aerial view shows the second half of the roof truss on the big red crane a day later.

They have gone 62 consecutive days without a safety incident. Roughly, there have been 185 craft workers on site daily. That number will grow to 200 plus over the next several months as more square footage is added in which to work. Overall, there have been 271,045-worker hours onsite.

The concrete superstructure is nearing completion at the north end. 18,500 yards of concrete have been placed. A night shot shows the arena superstructure and tower cranes. It shows the concrete ring beam at the highest elevation and the steel framing which frames in the roof superstructure.

Images are included in the packet showing the roof trusses. These are first built on the ground and the green vertical members shown are stanchions that support the roof truss being built on site. They are 36 feet deep and is all constructed on site before half is hoisted into place at a time. There are a couple of shots showing the first roof truss erection with workers guiding in the truss erection. In addition to the roof, work continues on other parts of the arena. They have included an image showing the ironworkers detailing the steel structure. They continue work on slab on metal deck and another picture shows the ironworkers installing rebar. The masonry work is shown on the freight elevator, which is in the northwest corner. The freight elevator is the lifeblood of an arena as it allows materials to be taken up and down so it is definitely a priority. Seedorf is shown completing the upper levels of the freight elevator. Exterior stud work is continuing on site, and there is a photo of the metal studwork being completed by Falewitch, the local union drywall company.

Clare asked if the arena was on schedule. Hinshaw confirmed, stating that weather has been great and, as Yancey mentioned, BNSF track removal was done early. All the foundations have been installed on the east and west side so the arena is complete with the deep foundations, the 100 foot deep piles are all installed, the concrete foundations have all be installed, and on the west side one level is up already.

## **5. Approval of Payment Registers**

Steve Hubka presented the Payment Registers for the month of July with total expenses, including Engineering charges, of \$7,159,000. One item of note is the \$1.2 million purchase of the Iron Horse Parking Lot. The remainder of expenses is similar to past expenses.

Clare asked if everything was on budget and looking good from an audit perspective. Hubka stated that it was.

Being no public comment, Carroll moved for approval of the payment registers. Clare seconded the motion. Motion carried 3-0.

## **6. Review of the July 2012 Expenditure Reports**

Steve Hubka presented the Job Cost Report for the capital budget and operating budget through July 31, 2012. There were no unusual items included in that timeframe.

Beutler asked for public comments. No one came forward.

**7. WH 12-49 Resolution to approve the Third Amendment to Exchange Agreement between the West Haymarket Joint Public Agency and BNSF Railway Company**

Rick Peo, City Attorney, explained that this is a catch up amendment to bring us up to date with purchase of BNSF property. Basically, this agreement does three things:

- 1) It acknowledges that we didn't go through the fourth closing, instead using right-of-entry agreements to get in and do the work. So this will combine the fourth and fifth closings to be the final closing with BNSF.
- 2) During the conveyances, one legal description was omitted of an easement to be conveyed to BNSF at 2<sup>nd</sup> and 'J' Street vacated property. This error will be corrected.
- 3) Finally, BNSF is acknowledging that Hill Street backout track, which is a storage track south of the West Haymarket area, is part of the original \$44 million contract. So that won't be an additional cost to the City. However, there is acknowledgement that the cost of any easements they have to pay to acquire the right-of-way from the City of Lincoln is a JPA reimbursable expense. If all fees were generated that total cost might be \$39,000.

Clare asked if this would conclude all the transactions relative to real estate with BNSF. Peo confirmed that was correct.

Hearing no public comment, Clare moved for approval of the resolution. Carroll seconded the motion. Motion carried 3-0.

**8. WH 12-50 Resolution approving the adoption of the 2012-2013 JPA Operating Budget**

Steve Hubka, City Finance, explained that this was advertised in Sunday's paper as the public hearing on the budget.

The operating budget for the JPA is anticipated to be \$15.7 million, of which \$15.1 million is debt service on the \$300 million of bonds that are outstanding. The interest expense goes up \$900,000 for the upcoming year because the last bond issue did not have a full year of interest that was payable in the current year. One significant decrease is due to advances to the District Energy Corp. which are no longer made as they have issued their debt. So, approximately \$950,000 goes away in this budget. There are some adjustments to various line items, such as City Treasurer cost reduction. But, there was an Accountant effort cost increase, due to time spent on Occupation Tax collection. We are currently taking in Occupation Tax at a rate of \$12.4 million per year, which is significantly higher than initial projections.

Clare asked if the sources shown at 11-8 are actually extremely conservative. He thought the tax coming in was much higher than what is eluded to in this document. Hubka confirmed that was correct, reiterating that current Occupation Tax is coming in at \$12.4 million.

Hearing no public comment, Carroll moved for approval of the 2012-2013 JPA Operating Budget. Clare seconded the motion. Motion carried 3-0.

**9. Set Next Meeting Date: Friday, September 7, 2012 at 3:00 p.m. in City Council Chambers Room 112**

Clare will be out of town on the Friday, September 7 meeting date, so an alternative date will be scheduled. (The new meeting date is Thursday, September 6 at 3:00 p.m. in Room 303.)

**10. Item 6 -- Motion to Adjourn**

Clare made a motion to adjourn the meeting. Carroll seconded the motion. Motion carried 3-0. The meeting adjourned at 3:22 P.M.

**Prepared by: Pam Gadeken, Public Works and Utilities**



**Pinnacle Bank Arena**



**Pinnacle**



**Pinnacle Bank Arena**

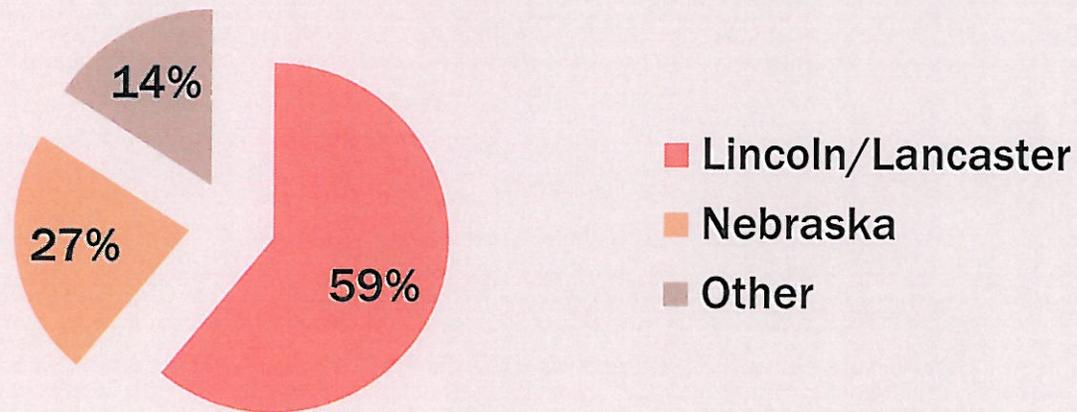
# **PINNACLE BANK ARENA**

**& Associated Infrastructure  
Projects**

**Project Status Update  
August 17th, 2012**

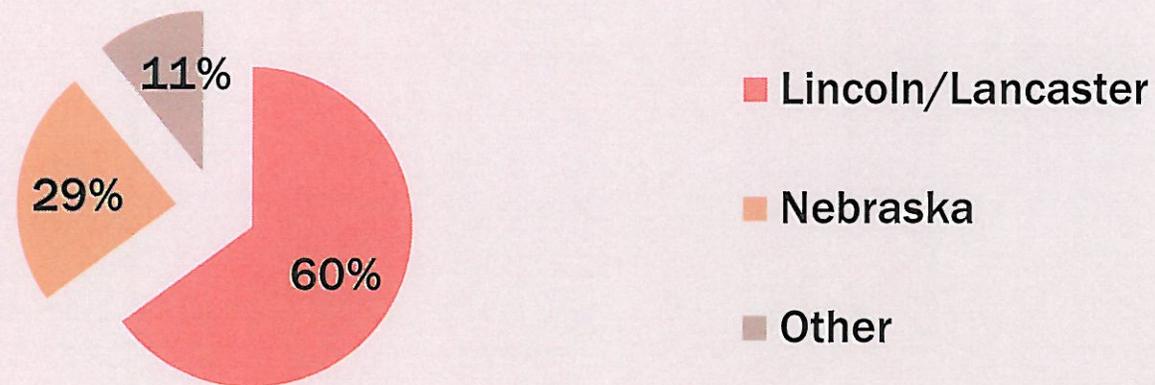
**WEST HAYMARKET LOCAL PARTICIPATION REPORT  
ALL WORKERS - INCLUDING ENGINEERING AND PROFESSIONAL SERVICES  
THROUGH JUNE/ JULY 2012**

	<b>Total</b>	<b>Percentage</b>
<b>Lincoln/Lancaster County</b>	<b>845</b>	<b>59%</b>
<b>Nebraska</b>	<b>397</b>	<b>27%</b>
<b>Other</b>	<b>206</b>	<b>14%</b>
	<b>1448</b>	<b>100%</b>



# WEST HAYMARKET LOCAL PARTICIPATION REPORT DAVIS BACON / CONSTRUCTION EMPLOYEES THROUGH JUNE/ JULY 2012

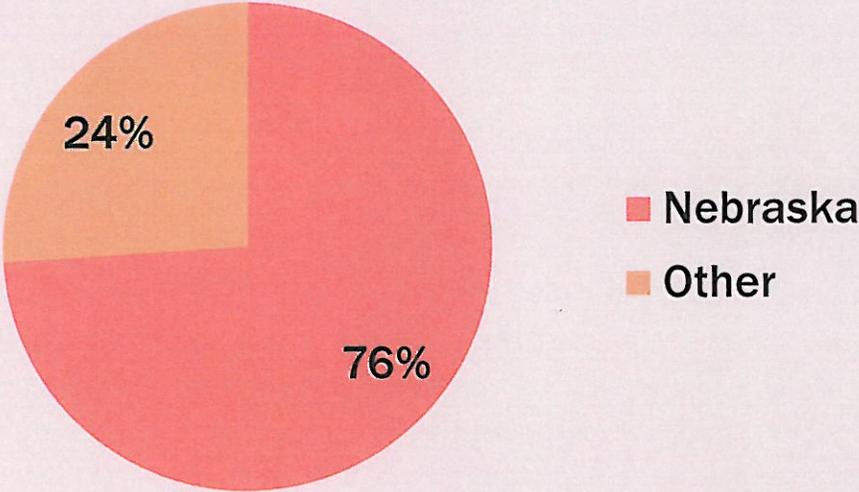
	Total	Percentages
Lincoln/Lancaster County	665	60%
Nebraska	326	29%
Other	118	11%
	1109	100%



# WEST HAYMARKET COMPANY PARTICIPATION

	Total	Percentage
Nebraska Firms	178	76%
Other	56	24%
	<del>175</del> 234	100%

\$136 Million  
Nebraska Dollars  
out of \$184 Million



# BNSF TRACKS BEING REMOVED FROM EAST SIDE OF ARENA



# BNSF CREWS REMOVING TRACKS NORTH OF THE ARENA



# BNSF 'X YARD' TRACKS MOSTLY REMOVED ON WEST SIDE OF ARENA



# BNSF MAIN LINES 1 AND 2 BEING REMOVED IN FRONT OF HISTORIC CANOPY



# BNSF 'X YARD' TIES BEING STOCKPILED FOR REMOVAL BY BNSF CREWS



# N STREET OPENED TO TRAFFIC BETWEEN 8<sup>TH</sup> & 9<sup>TH</sup> STREETS



# N STREET OPENED TO TRAFFIC BETWEEN 7<sup>TH</sup> & 8<sup>TH</sup> STREETS



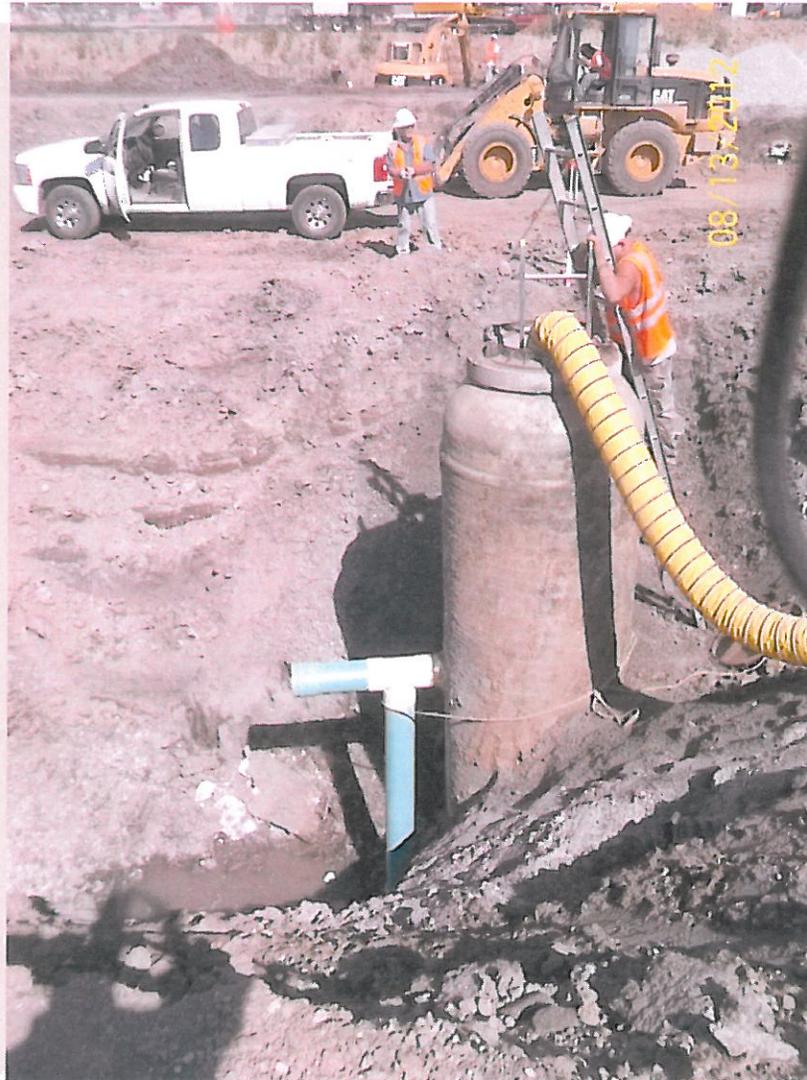
# CONSTRUCTORS CREW PAVING FINAL CORNER OF 7<sup>TH</sup> & N INTERSECTION



# FORMER WATSON BRICKSON SITE



# TAPPING SANITARY MANHOLE ALONG PINNACLE ARENA DRIVE



# CONSTRUCTING SEWER PIPE JUNCTION ALONG PINNACLE ARENA DRIVE



# PICKING UP CONCRETE STORM SEWER PIPE ALONG PINNACLE ARENA DRIVE



# REMOVING ROOF PANELS FROM HISTORICAL CANOPY



# SECTION OF HISTORICAL CANOPY REMOVED TO ALLOW FOR Q ST. SEWER WORK

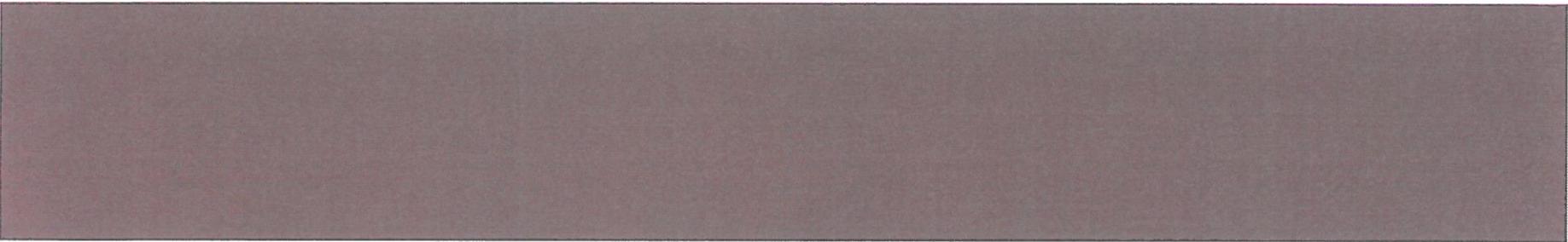


# JUDDS BROS. REMOVING 'WYE' TRACKS, ADJUSTING GRADE



# ALL 'WYE' TRACKS REMOVED





# **Pinnacle Bank Arena Progress Update**

# AERIAL AUGUST 15, 2012



# AERIAL AUGUST 15, 2012



# Safety

- Worked 62 consecutive days without a safety incident
- The Project averages over 185 Craft Workers daily
- The Project has worked 271,045 hours on site

# Concrete Superstructure

- The concrete work continues with Slab on Grade, Slab on Metal Deck, and Elevated Beam Pours
- 18,500 yards of concrete have been placed.



# SUPERSTRUCTURE



# ROOF STEEL - 1<sup>ST</sup> TRUSS



# 1<sup>ST</sup> TRUSS ERECTION



# 1<sup>ST</sup> TRUSS ERECTION



# IRONWORKERS SETTING 1<sup>ST</sup> TRUSS



# DETAILING STRUCTURAL STEEL



# INSTALLING REBAR



# MASONRY WORK AT FREIGHT ELEVATOR



# INSTALLING EXTERIOR STUDS

