

LINCOLN CITY/LANCASTER COUNTY PLANNING STAFF REPORT

for October 12, 2016 PLANNING COMMISSION MEETING

*** REVISED NOVEMBER 1, 2016 ***

- PROJECT #:** Use Permit No. 16009 - Costco Center
- PROPOSAL:** Approval of a use permit for a 199,000 square foot commercial center.
- LOCATION:** South 14th Street and Pine Lake Road
- LAND AREA:** 40.58 acres, more or less
- EXISTING ZONING:** AG Agriculture District and R-1 Residential District
- CONCLUSION:** If the associated comprehensive plan amendment and change of zone applications are approved, this site is appropriate for commercial development as proposed, dependent upon the additional screening, trail crossing improvements, and other conditions of approval. This request complies with the Zoning Ordinance and is consistent with the Comprehensive Plan.

RECOMMENDATION:	Conditional Approval
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GENERAL INFORMATION:

LEGAL DESCRIPTION: See attached legal description.

EXISTING LAND USE: Agriculture and funeral home

SURROUNDING LAND USE AND ZONING:

North: Lincoln Memorial Park cemetery and agricultural; R-1
South: Commercial; B-2
East: Scott Middle School; P
West: Lincoln Southwest High School and single family residential; I-3 and AG

ASSOCIATED APPLICATIONS: Change of Zone #16030 and Comprehensive Plan Amendment #16004

HISTORY:

Prior to 1979, this area was zoned A-1 Single Family Dwelling District. In 1979, the zoning was changed to R-1 Residential District. The western portion of the site was re-zoned to AG Agriculture some time after 1979.

- Apr 1987 The Lincoln Memorial Park cemetery was designated as Pre-Existing Special Permit #12 with approval of Administrative Amendment #87002 to add a mausoleum.
- May 1989 Administrative Amendment #89093 to Pre-Existing Special Permit #12 approved four signs on the west side of the Lincoln Memorial Park property.
- Sep 1996 Administrative Amendment #96036 to Special Permit #1570 for The Ridge 1st Addition Preliminary Plat and South Ridge Community Unit Plan showed a conceptual future lot layout for single family residential homes on the east portion of the site currently zoned R-1 Residential. That area was platted as an outlot for future development in August 1996 with The Ridge 13th Addition.
- Jun 1999 Administrative Amendment #98062 to Pre-Existing Special Permit #12 for Lincoln Memorial Park approved a manufactured structure to be used as an office.

COMPREHENSIVE PLAN SPECIFICATIONS:

P. 1.9 - The Future Land Use Map currently designates this area for future Public and Semi-Public and Urban Density Residential land uses. The associated application for Comprehensive Plan Amendment #16004 would revise the future land use on the majority of this site to Commercial.

P. 5.1 - Focus primarily on retention and expansion of existing businesses; attracting new businesses should also be encouraged.

P. 5.1-5.2 - Business & Economy Guiding Principles

- Focus primarily on retention and expansion of existing businesses; attracting new businesses should also be encouraged.
- Promote and foster appropriate, balanced, and focused future economic growth that maintains the quality of life of the community.
- Seek to efficiently utilize investments in existing and future public infrastructure to advance economic development opportunities.
- Provide flexibility to the marketplace in siting future commercial and industrial locations.
- Strive for predictability for neighborhoods and developers.
- Encourage commercial centers to encompass a broad range of land uses with the integration of compatible land use types.

P. 5.3 - Apply design standards as a tool for economic development. They provide assurances for surrounding property owners as well as prospective developers.

P. 5.5 - Commercial and Industrial Development Strategies

- It is the policy that Commercial and Industrial Centers in Lancaster County be located:
 - Within the City of Lincoln or incorporated villages.
 - Outside of saline wetlands, signature habitat areas, native prairie and floodplain areas (except for areas of existing commercial and industrial zoning).
 - Where urban services and infrastructure are available or planned for in the near term.
 - In sites supported by adequate road capacity — commercial development should be linked to the implementation of the transportation plan.

- In areas compatible with existing or planned residential uses.
- In existing underdeveloped or redeveloping commercial and industrial areas in order to remove blighted conditions and to more efficiently utilize existing infrastructure.
- In areas accessible by various modes of transportation (i.e. automobile, bicycle, transit, and pedestrian).
- So that they enhance entryways or public way corridors, when developing adjacent to these corridors.
- In a manner that supports the creation and maintenance of green space as indicated in the environmental resources section of this Plan.

P. 5.7 - Strategies for Commercial Centers

- Disperse Commercial Centers throughout the community to support convenience of access and to lessen impacts on infrastructure.
- Locate Commercial Centers where they will have access to arterial streets with adequate capacity and be supported by transit, trails, sidewalks, and local streets.
- Discourage "four corner commercial development."
- Encourage multiple street connections to adjacent residential neighborhoods to allow convenient access for neighboring residences and pedestrians without the use of arterial streets, but exercise care in designing the street network to minimize undesirable traffic impacts.
- Develop smaller stores next to larger anchor stores in centers to encourage small businesses and to provide a variety of goods and services for customers utilizing the centers.
- Design streets and public spaces to enhance pedestrian activity and support multiple modes of transportation.
- Create a pedestrian-oriented environment in the physical arrangement of buildings and parking.
- Develop Commercial Centers as compact clusters or mixed use nodes with appropriate site design features to accommodate shared parking and ease of pedestrian movement, to minimize impacts on adjacent areas, and encourage a unique character.
- Discourage auto-oriented strip commercial development; Commercial Centers should not be developed in a linear strip along a roadway or be completely auto-oriented.
- Design new Commercial Centers in a manner that facilitates future development and intensification of land uses on the site.
- Redevelop existing commercial strips for residential mixed use and/or transit oriented development where appropriate.
- Design buildings and land uses at the edge of the center to be compatible with adjacent residential land uses. Examples of compatible land uses include apartments, mixed use residential buildings, offices, assisted living facilities, or child care centers.
- Buildings should be compatible in terms of height, building materials and setback. Small compatible commercial buildings at the edge could include retail or service uses. Buildings with more intrusive uses should have greater setbacks, screening requirements and be built of more compatible materials.
- Locate the most intensive commercial uses, such as restaurants, car washes, grocery stores, gas stations/ convenience stores and drive through facilities nearer to the major street or roadway and furthest from the residential area (unless contained within a mixed use center). Lighting, dumpsters, loading docks and other service areas should be screened from residences.

UTILITIES: All utilities are available.

TOPOGRAPHY: The site currently slopes down approximately 20 feet from north to south.

TRAFFIC ANALYSIS: South 14th Street is classified as a principal arterial, Pine Lake Road is classified as a minor arterial, and Hazel Scott Drive is classified as a local street in the 2040 Comprehensive Plan.

PUBLIC SERVICE: The nearest fire station is located at South 27th Street and Old Cheney Road.

ANALYSIS:

1. This request is associated with the comprehensive plan amendment and change of zone for the Costco Wholesale Use Permit. The appropriateness of the overall commercial use is addressed in the reports on those applications.
2. Revised versions of Sheets 1 through 5, landscape plans, and revised stormwater documents were submitted prior to the November 9th Planning Commission meeting. This staff report, including the conditions of approval, has been revised to address the changes. A number of conditions have been removed that were addressed in the re-submitted plans. Many of the conditions remain the same that were not addressed by the applicant. There are also several new conditions to address elements of the plan that were removed in the re-submittal.
3. The site plans show six commercial lots being created, with an outlot for detention in the southeast corner of the development. A use permit is proposed for up to 199,000 square feet of commercial floor area distributed over the six lots. Access is provided via South 14th Street, Pine Lake Road, and Hazel Scott Drive.
4. It was originally stated that a waiver to Section 7.5 of the Design Standards for Screening and Landscaping was necessary to waive screening between the Lincoln Memorial Park funeral home and the rest of the Lincoln Memorial grounds to the north. However, it was determined a waiver was not needed because the character of the uses would not be substantially different to require screening. Therefore, there are no waiver requests associated with this application. Several requests for deviation from the Access Management Policy will be required.
5. This site has long been shown as primarily future Public & Semi-Public land uses in considering its ownership under Lincoln Memorial Park in anticipation of future expansion of the cemetery. The area on the east side of the site had been conceptually shown in 1996 as a continuation of the single-family neighborhoods to the north, as part of Administrative Amendment #96036 to Special Permit #1570 for The Ridge 1st Addition Preliminary Plat and South Ridge Community Unit Plan. However, no definite plans or layout were ever approved. The land was then sold to the cemetery and could be used for cemetery expansion.
6. With Lincoln Memorial proposing to sell the undeveloped land for development rather than retain it for cemetery expansion, it was necessary to consider the proposed commercial uses and their appropriateness and suitability for this site. Due to the location along two major streets and the existing commercial uses across Pine Lake Road, commercial development on the site is appropriate given the conditions of approval that address various impacts. A buffer area of R-1 residential

zoning would remain between the commercial area and existing homes to the north; that area will continue to be shown for future residential land uses. The use permit boundary is approximately 600 feet from existing homes to the northeast.

7. A number of design elements and conditions of approval are proposed to mitigate the effects of traffic, noise, and visual impacts on nearby residential, school, and cemetery land uses. These include site-specific design standards and facade elevations, additional landscape screening, trail crossing improvements, provision of an alternative pedestrian route around the back of the Costco site, and use restrictions on the northeast commercial pad site.
8. The exterior design of the Costco building was considered an important element from the standpoint of the City and neighbors. Proposed elevations for the Costco building and fuel facility are included as part of the official use permit application. The building elevations will be required to substantially comply with the submitted elevations as modified by the conditions of approval at the time of building permit.

In addition, basic design standards that will apply to the use permit area as a whole, including the Costco building, are proposed by the Planning Department. The applicant does not object to implementing design standards for the commercial center, but the applicant shows a revised version of the standards on their site plan as compared to the standards proposed by the City.

The design standards (as modified by the conditions of approval) would require that buildings of 50,000 square feet or greater will include at least 20% of the gross area of vertical surfaces as decorative concrete masonry units (CMU), brick or stone; half of those required masonry materials shall be brick or stone, the remainder and any masonry areas in excess of 20% may be decorative CMU. Buildings of less than 50,000 square feet will include at least 50% of the gross area of vertical surfaces as decorative CMU, brick or stone; half of those required masonry materials shall be brick or stone, the remainder and any masonry areas in excess of 50% may be decorative CMU. The required masonry materials must be used to the extent feasible on facades oriented towards public streets, with any remaining required masonry used on other facades. All buildings in the center must utilize building materials and colors similar to the Costco building.

Costco has proposed revisions to the design standards that would eliminate any minimum percentage requirement of brick or stone on the main building while reducing (but retaining) a requirement of those materials on smaller buildings in the development. Costco's proposed change would substantially lessen the material continuity between structures within the project because the Costco building would not have any percentage of masonry as brick or stone, while the other buildings in the center would be required to have a minimum percentage.

The design standards will help ensure that the center maintains a cohesive look and a higher-quality architectural appearance over time. The requirement to use some brick or stone on the Costco building provides at least a degree of continuity between this very large and prominent building and other commercial and large institutional structures in its vicinity. The smaller module of bricks, as compared to Concrete Masonry Units, also provides an element of more human scale to the long facades, especially when approaching or entering the building. Retaining a minimum percentage of brick or stone is required as a condition of approval to ensure a cohesive appearance within the center.

9. The following landscape screening is required according to the Design Standards for Screening and Landscaping:
 - a. Section 7.1 - Parking Lots, Parking Areas, and Driving Aisles
Requires screening from the ground to 3 feet around parking lots/areas and one shade tree per 6,000 square feet of parking lot paving.
 - b. Section 7.5 - Adjacent Land Uses and Zoning Districts of a Substantially Different Character Abutting Residential Districts
Requires a 60% screen from the ground to 10 feet. This standard would apply along the north side of the use permit except along the north side of Lot 1 for the funeral home.
 - c. Section 7.6 - Around Proposed Buildings in the B-2 District
Requires a minimum 3 deciduous shade trees and 400 square feet of ornamental grass or shrub coverage per 10,000 square feet of building coverage.
 - d. Section 7.11 - Outdoor Refuse Areas, Recycling Bins, Open Storage, Loading Areas, and Ground Level Mechanical Equipment
Requires screening from the ground to 10 feet if located within 150 feet of a street right-of-way and within public view or abutting a residential district.
10. In order to mitigate the impact of this request, the following additional landscaping will be required for this application. The additional screening on the north and east sides of the site is intended to reduce the impact of the development on the nearby cemetery, school, and residential uses. The additional screening on the south side along Pine Lake Road is intended to mirror the plantings installed for the commercial area south of Pine Lake Road and contribute to improving the overall appearance and screening of the large parking lots for Costco and the other commercial development that will be adjacent to the trail and street. The south side screening also mitigates the long blank Costco facade facing Pine Lake Road.
 - a. North Side - Screening is required as a condition of approval along the entire north side of the site with the exception of the north side of Lot 1, to be

screened with a 30-foot wide landscaped buffer zone with a 100% landscape screen from the ground to 10 feet. In the locations where a berm will be incorporated, the berm height may count towards the screening requirement. This screening requirement may count towards and will exceed the minimum requirement under Section 7.5 of the Design Standards listed above. This screening is required to be installed with construction on Lot 5.

- b. East Side - Screening is required as a condition of approval along the east side of Lot 6 and on Lot 5 east of the parking lot adjacent to the west side of Outlot "A" that meets the standard set forth in Section 7.5 of the Design Standards. This screening is required to be installed with construction on Lot 5.
- c. South Side - A 60% landscape screen from the ground to 6 feet is required as a condition of approval on the south side of Lots 2 through 5. This screening is required to be installed at the time of building permit for each lot. Screening on the south side will be required over and above the street tree requirement and parking lot screening requirements. Street trees on South 14th Street, Pine Lake Road, and Hazel Scott Drive will be required at the time of final platting. Street trees may be installed behind the sidewalks/trail. Some of the street trees and portions of the trail would be located on private property with the proposed site plan, and would be the applicant's responsibility to maintain.

Landscape plans for the overall site and two cross-sections of the 30-foot landscape buffer/berm were submitted with the revised plans. The height of the berm/grade change varies from 8-10 feet at the center of the site to 4-5 feet towards the west. The berm/grade change may count towards the screening requirement. The submitted landscape plans do not fully meet all minimum screening requirements or the additional screening requirements. Therefore, the minimum required and proposed additional screening with this application, that is not shown on the landscape plans, is added as a condition of approval.

The additional landscaping requirements proposed to address the impact of this change in land use have not changed. At the request of the applicant, the screening on the east side of Outlot "A" was allowed to be installed instead on Lot 5 east of the parking lot and adjacent to the west side of Outlot "A". The applicant requested this change to allow greater visibility to Lot 6 and it should not negatively impact the adjacent uses. Landscaping will be submitted and approved as part of the building permit process on each lot. However, the full landscaping on the north and east sides of the site is required as a condition of the Use Permit to be installed at the time of building permit on Lot 5 to ensure that screening is in place regardless of when Lot 6 develops.

11. Portions of the existing trail on the north side of Pine Lake Road will be relocated north due to installation of right turn lanes at the two proposed driveways on Pine Lake Road at South 16th Street and South 18th Street. Several trail crossing improvements are proposed to increase safety for pedestrians and bicyclists. The developer is showing several safety measures at the 18th Street crossing only. A condition of approval for this application would modify the trail crossing improvements to show all of the following safety measures at both the 16th Street and 18th Street crossings:
 - a. Stop signs for vehicles turning right out of the driveways, but no stop signs on the trail as trail users have the right-of-way;
 - b. Diverting the trail crossings an additional five feet to the north than is shown on the site plan to further alert and slow trail users; this measure would also increase storage of a single vehicle between the trail crossing and edge of travel path for westbound vehicles on Pine Lake Road;
 - c. Marked stop bars for vehicles turning right out of the driveways;
 - d. Installation of advanced trail crossing signs along the driveways approaching Pine Lake Road to alert drivers to the upcoming trail crossing;
 - e. The trail crossings must be on raised traffic tables; and
 - f. The trail crossings must be marked/striped.

The site plan shows stop signs and stop bars on the trail improvement inset. The City recommends those not be installed, as both bicycle and pedestrian trail users have the right-of-way over vehicles and should not be prompted to stop. The revised site plans do not show the crossings on raised traffic tables. Although the site will slope down from north to south, there should still be opportunity to implement a higher grade on the trail crossings as compared to the driveways, while still allowing for adequate storm water flow.

12. At the request of neighbors and in line with the recommendation of the Pedestrian/Bicycle Advisory Committee, an alternative route to avoid the Pine Lake Road trails crossings has been provided in the form of a 5-foot private sidewalk. The sidewalk would run around the north and west sides of the Costco site from Hazel Scott Drive down to Pine Lake Road on the west side of the 16th Street driveway.
13. This project would be located between two schools, Lincoln Southwest High School and Scott Middle School. Adams Elementary is located in the neighborhood south of Pine Lake Road between South 14th and South 27th Streets. Based on the Traffic Impact Study, the proposed 5-foot private sidewalk around the site, and the trail

crossing improvements set out in the conditions of approval, the proposed plan addresses potential impacts on school children and school-related vehicular traffic. The proposed trail crossing measures are in line with other similar driveway and street crossings in Lincoln. The removal of the north driveway on Hazel Scott Drive that was shown on the previous plans should help alleviate concerns expressed about loss of the informal parking by parents along either side of Hazel Scott Drive for pick-up/drop-off and special events at the middle school. Restricting the Hazel Scott Drive access to right-out/left-in only as shown should help eliminate some minor amount of traffic that might otherwise cut through the neighborhood going to and from the stores. The developer will also be required to build the missing sidewalk connection on the north side of Hazel Scott Drive from the Costco development to Shadow Ridge Road.

14. Use restrictions on Lot 6, the northeast pad site, are proposed in response to concerns expressed by neighbors about potential impacts of undesirable, disruptive, and/or 24-hour uses on the site per the B-2 zoning if restrictions were not put in place. Lot 6 is the closest lot to existing homes to the northeast. It is proposed to prohibit fast food drive thru facilities, motorized vehicle fuel sales facilities, motorized vehicle repair/service, motorized vehicle wash facilities, and the sale of alcoholic beverages for consumption off the premises (liquor sales). Bars would also be prohibited; however, consumption of alcohol on the premises in a restaurant would still be possible on Lot 6 as long as spacing and other state licencing requirements were met.
15. There was concern expressed by the public about impacts to the Lincoln Memorial Park cemetery grounds to the north. As discussed above, a 30-foot wide landscaped buffer would be installed on the north side of the Costco property and the north side of Lot 6. The landscape buffer will provide visual and noise screening to the cemetery. The distance from the north side of the buffer to the edge of the cemetery grounds is variable but would be approximately 137 feet at the closest point, which is on the western end adjacent to the funeral home (see attached informational exhibit titled "Cemetery Distance Exhibit" included with the site plans). On the eastern end, there would be approximately 257 feet between the north side of the buffer and the cemetery grounds. In addition, the land in between will be retained by Lincoln Memorial Park and would be re-zoned from AG to R-1 with the associated change of zone application to indicate that commercial development is not appropriate there.
16. A formal Traffic Impact Study (TIS) was prepared by the developer's project engineer which detailed the site specific impacts and mitigation strategies for the proposed Costco development. The TIS was conducted by licensed professional traffic engineers and followed the City of Lincoln process for TIS requirements as presented in the City's Access Management Policy (AMP). This is the same process that is followed for all new development and redevelopment projects to identify operational and safety improvements that are required to best mitigate

expected impacts of additional trips on the City street network.

The TIS was reviewed by the Public Works and Utilities Department, who found it generally satisfactory. The scope of the study included an evaluation of existing traffic conditions at study area intersections and also the proposed traffic conditions with buildout of the site and commercial land uses as detailed in the Institute of Transportation Engineers Trip Generation Manual, Latest Edition. The TIS analyzed worst-case peak hour turning movement volumes that consider both the application of new, estimated vehicle trips in addition to peak hour commuter traffic at study area intersections. The traffic study included anticipated trip generation for the four pad sites adjacent to their project. The assumed uses for the pad sites were reasonable and should provide a conservative estimate of ultimate site trips. Key highlights and recommendations of the TIS include the following:

- a. Recommendation of increased level of traffic control at the intersection of Pine Lake Road and South 20th Street/Hazel Scott Drive including either a roundabout or a signalized intersection with improved turn lane geometry on all approaches.
- b. A median-divided, $\frac{3}{4}$ access (left-in, right-in/right-out) at the intersection of Pine Lake Road and South 16th Street for improved operations and safety.
- c. Right-in/right-out access locations along both South 14th Street and Pine Lake Road with auxiliary turn lanes.
- d. Extension of several existing study area turn lanes and modifications to the South 14th Street and Pine Lake Road traffic signal and geometry.
- e. Improvements to alignment and markings of the existing trail crossing at site access locations and additional sidewalk route along the rear of the proposed development.

Public Works recommends several revisions to the site plan based on the traffic information submitted, which are listed in the conditions of approval below. The primary revisions include removal of the traffic signal shown at South 16th Street and providing for the removal of the south driveway on South 14th Street at the time the funeral home redevelops into another commercial use.

A condition of approval would require a common access easement be dedicated through the funeral home parking lot and over the two access points on South 14th Street. It is important that traffic for the new commercial uses have access to the South 14th Street driveway through the funeral home parking lot to allow some left-in movements from 14th Street that avoid the 14th Street and Pine Lake Road intersection. The funeral home could temporarily block access through their site during funeral services. Another condition requires that the funeral home's south

driveway be eliminated at the time the funeral home redevelops into another commercial use. The south driveway access does not meet Access Management Policy and should be eliminated in the future, given that the north driveway will then be extended east to more directly serve the rest of the commercial center.

Several major traffic-related changes were included in the re-submitted plans. First was the removal of the north driveway access on Hazel Scott Drive. Left-out and right-in turns are shown as prohibited from the driveway on Hazel Scott Drive. Public Works does not oppose this change, which was added at the request of neighbors who were concerned about traffic that might exit left from the site or enter via Hazel Scott Drive and increase traffic through the neighborhood and around the north side of the school.

The site plan continues to show a traffic signal symbol at South 16th Street and Pine Lake Road. In addition, Sheet 2 now shows three "Intersection Design Options" at South 16th Street including a fully open option with a traffic signal, a fully open option with no signal, and three-quarter access (no left outs permitted). A traffic signal and fully open intersection without signal are not recommended or supported by the TIS. A signal at that intersection would not meet the Access Management Policy spacing requirements, and it is unlikely that such a signal would meet the minimum traffic warrants necessary to justify it. Public Works and Utilities continues to be supportive of the three-quarter access only at that intersection as recommended by the TIS.

If a traffic signal were installed at South 16th Street, then the signal at Hazel Scott Drive would likely be deleted as described in the conditions of approval. Two traffic signals along Pine Lake Road for this development are not necessary or justifiable based on the traffic generation projected for this application. This would increase wait times for school and neighborhood traffic. If a signal were approved at 16th Street, the developer would be responsible for the cost of the signal. The developer could request an improvement district be approved in cooperation with the commercial development to the south that would also benefit from the signal. The City does not have funding available to put towards installation of a signal at South 16th Street.

The site plan no longer shows future potential right-of-way to be dedicated in the event of roundabout construction at South 20th Street/ Hazel Scott Drive and Pine Lake Road. One of the conditions of approval would require that the right-of-way be added back onto to the site plan, with the understanding that the right-of-way would be dedicated in the event the signalized intersection is converted to a roundabout some time in the future. Providing for the future roundabout is an important consideration in approval of the overall plan.

17. Parking will be regulated per Chapter 27.67 of the Zoning Ordinance. No parking waivers are requested. The standard parking requirement for office and commercial uses in the B-2 District is 1 stall per 300 square feet.
18. Stormwater detention will be provided in the southeast corner of the site. Title 28 on Stormwater Quality is applicable for this project because there is no approved preliminary plat and the area of construction activity is greater than one acre.
19. Neighborhood meetings were held on August 31 and September 27, 2016. Both meetings had high attendance by neighbors, nearby commercial property owners, and other interested parties. A significant number of written comments were received prior and subsequent to the official application submittal. The Planning Commission has received copies of all written comments received on the Costco project and applications.

CONDITIONS OF APPROVAL:

This approval permits 199,000 square feet of commercial space.

Site Specific Conditions:

1. The City Council approves associated request:
 - 1.1 Change of Zone #16030
 - 1.2 Amendment of Comprehensive Plan #16004
2. The permittee shall cause to be prepared and submitted to the Planning Department a revised and reproducible final plot plan including **3** copies with all required revisions and documents as listed below before a final plat is approved:
 - 2.1 Make the following revisions on all applicable sheets:
 - 2.1.1 At the intersection of Hazel Scott Drive/South 20th Street and Pine Lake Road:
 - 2.1.1.1 Revise the north and south approaches to include a single lane departing Pine Lake Road and three lanes approaching Pine Lake Road. This is consistent with the mandatory four-in cross section for a signalized intersection, side-street approach design.
 - 2.1.1.2 Show extended east/westbound turning lanes as recommended by the Traffic Impact Study.

- 2.1.1.3 Show right-of-way for the future roundabout on a separate outlot and revise the grading plan accordingly.
- 2.1.2 On Hazel Scott Drive:
 - 2.1.2.1 Show a 265-foot plus taper southbound right turn lane at Hazel Scott Drive and Pine Lake Road. The proposed exiting driveway should not conflict with this turn lane.
 - 2.1.2.2 Revise the note for the Hazel Scott Drive access to read, "Right-out and left-in only. No right-in or left-out."
 - 2.1.2.3 Extend the boundary of Outlot "A" east and south to the edge of the property adjoining Hazel Scott Drive and Pine Lake Road.
- 2.1.3 At the intersection of South 16th Street and Pine Lake Road:
 - 2.1.3.1 Retain $\frac{3}{4}$ -access as proposed and recommended by the Traffic Impact Study. Remove the three Intersection Design Options at the top of Sheet 2.
 - 2.1.3.2 Remove the traffic signalization symbol and the note for "Future signalized intersection".
 - 2.1.3.3 Show the vacation of part of the right-of-way stub on Private Drive A to the satisfaction of Public Works and Utilities.
- 2.1.4 At the right-in/right-out access point on South 14th Street, show a 165-foot plus 50-foot straight taper northbound right turn lane.
- 2.1.5 At the intersection of South 14th Street and Pine Lake Road:
 - 2.1.5.1 Modify the median in Pine Lake Road for South 14th Street and South 16th Street to provide left turn lane lengths of approximately 300' westbound and 200' eastbound, with a 120' taper for each direction to the satisfaction of the Public Works and Utilities Department.
 - 2.1.5.2 Show opening of the second southbound left turn lane and modifications to the signal for protected southbound left-turn phasing as recommended by the Traffic Impact Study.

- 2.1.6 Show standard taper lengths on all turn lanes. The only exception is for the northbound right turn lane into Private Drive West as identified in Condition 2.1.5.1 above.
 - 2.1.7 Show a common access easement over the funeral home driveways and parking lot to the proposed east-west private drive. Add a note to Sheet 1 stating that this access easement may be blocked during funeral services.
 - 2.1.8 Revise the proposed right-of-way/easement dedication shown for the trail along Pine Lake Road to more clearly differentiate between the proposed right-of-way and easement legend patterns. This information may be shown on an additional sheet if necessary.
 - 2.1.9 Revise the driveway entrance design on Hazel Scott Drive to clearly prohibit right-in turn movements, to the satisfaction of the Public Works and Utilities Department.
 - 2.1.10 The sidewalk to be installed along Hazel Scott appears to be shown against the right-of-way line. Revise placement of the sidewalk to be 1-2 feet off the right-of-way line in the event that the City needs to repair or replace the sidewalk in the future.
- 2.2 Make the following revisions to Sheet 1:
- 2.2.1 Revise Note 15 to read, "If the existing funeral home on Lot 1 redevelops into different commercial uses, the following is required: the northern driveway will be reconfigured to better serve the multiple commercial properties; a right hand turn lane will be required to the northern driveway on South 14th Street and the sidewalk relocated to provide adequate safety for pedestrians; all improvements, and right of way dedication, will be at the expense of the developer redeveloping the funeral home property; the south right-in/right-out access on South 14th Street shall be eliminated at that time; a 300-foot plus taper northbound right turn lane shall be required to be installed at that time for the north access point on South 14th Street; and a common access easement shall be provided over Lot 1 from South 14th Street along the new east-west driveway."
 - 2.2.2 Revise the last sentence of Note 16 to read, "10 feet of additional right-of-way or easement will be dedicated/provided." Add the following text at the end of the note, "The bike trail shall be located a minimum of 10 feet behind back of curb to provide adequate separation and room for street trees. Street trees shall be installed six feet from the back of curb."

- 2.2.3 Add the following note: “The future right-of-way shown for a roundabout at South 20th Street/Hazel Scott Drive and Pine Lake Road shall be dedicated if construction of a roundabout at that intersection occurs.”
- 2.2.4 Add the following note: “If a traffic signal is approved at the intersection of South 16th Street and Pine Lake Road, the following shall occur: the developer is required to submit a revised Traffic Impact Study that provides justification for the signal to the satisfaction of the Public Works and Utilities Department; in addition, the right-of-way stub on Private Drive “A” shall be retained by the City; the traffic signal at South 20th Street/Hazel Scott Drive and Pine Lake Road shall be removed from the plans. The cost to install the traffic signal will at the developer’s expense.”
- 2.2.5 Revise Note 18 to read, “The buildings on Lot 5 shall substantially conform to the building elevations included in this use permit (sheets 6 thru 10) to the satisfaction of the Planning Director.”
- 2.2.6 Revise the second sentence of Note 21 to read, “The Maintenance Agreement shall be submitted at time of building permit.”
- 2.2.7 Revise General Note 19 on Sheet 1 to read: “All buildings in the Center, including the Costco building, shall meet the following design standards:
- A.) Buildings of 50,000 square feet or greater will include at least 20% of the gross area of vertical surfaces as decorative CMU, brick or stone; half of those required masonry materials shall be brick or stone, the remainder and any masonry areas in excess of 20% may be decorative CMU. Buildings of less than 50,000 square feet will include at least 50% of the gross area of vertical surfaces as decorative CMU, brick or stone; half of those required masonry materials shall be brick or stone, the remainder and any masonry areas in excess of 50% may be decorative CMU. The required masonry materials shall be used to the extent feasible on facades oriented towards public streets, with any remaining required masonry used on other facades.
 - B.) All buildings in the center will utilize building materials and colors similar to the Costco building.

C.) Calculations of the gross area of vertical surfaces, the required area of masonry materials, and the proposed area of masonry on each facade shall be provided at the time of application for building permits.”

2.2.8 Add the following note: “The following additional screening requirements apply:

- A.) Screening is required along the east side of Lot 6 and on Lot 5 east of the parking lot adjacent to the west side of Outlot “A” that meets the standard set forth in Section 7.5 of the Design Standards for Screening and Landscaping. This screening is required to be installed with construction on Lot 5.
- B.) Screening is required on the south side of Lots 2 through 5 to provide a 60% landscape screen from the surface of the ground to 6 feet. This screening is required to be installed at the time of building permit for each lot.
- C.) A 30-foot wide landscaped buffer zone is required along the north side of Lots 5 and 6 to provide a 100% landscape screen from the surface of the ground to 10 feet. This screening is required to be installed with construction on Lot 5.
- D.) The north side of Lot 1 does not need to comply with Section 7.5 of the Design Standards for Screening and Landscaping until such time as the funeral home redevelops into a different use.”

2.2.9 Correct the Required Parking Stalls for Lot 1 to 74 and revise the Total Required Parking Stalls to 667.

2.3 Make the following revisions to Sheet 3:

2.3.1 Specify that the “Trail Crossing Detail” applies to both the South 16th Street and South 18th Street trail crossings.

2.3.2 Modify the Trail Crossing Detail to show only the following safety improvement measures:

2.3.2.1 Stop signs for vehicles turning right out of the driveways.

2.3.2.2 Marked stop bars for vehicles turning right out of the driveways.

- 2.3.2.3 Advanced trail crossing signs along the driveways approaching Pine Lake Road.
- 2.3.2.4 The trail crossings shall be on raised traffic tables to the satisfaction of the Public Works and Utilities Department.
- 2.3.2.5 The trail crossings shall be marked/striped.
- 2.3.3 The sidewalk connection from Pine Lake Road to the store should be shown as clearly connecting to the trail along Pine Lake Road per the Design Standards for Pedestrian Circulation in Commercial and Industrial Areas, Section 2, item #1.
- 2.3.4 Note that truck deliveries may not exit/enter from Hazel Scott Drive.
- 2.4 Revise the title of Sheet 8, "Loading Dock Perspective" as the loading dock does not appear on the elevation.
- 2.5 Identify the trees northeast of the development as to remain during construction.
- 2.6 Submit a revised Grading & Drainage Plan, stormwater report, and any other necessary drainage-related plans and calculations to the satisfaction of the Public Works and Utilities Department. No grading anywhere on the site is permitted to begin until this condition of approval is complete.
- 2.7 Revise the submitted elevations as necessary to meet the design criteria as modified with Condition of Approval 2.2.7.
- 2.8 Revise the submitted elevations for the Costco warehouse building to provide curbs between pedestrian areas and parking or driving aisles as required in the Design Standards for Pedestrian Circulation in Commercial and Industrial Areas.
- 2.9 Provide two cross-sections of the 30-foot landscape buffer per Condition 2.2.8 to the satisfaction of the Planning Director, to be included as an official sheet in the use permit plan set.
- 2.10 Revise the submitted landscape plans as necessary to meet the minimum landscape design standards and the additional required screening as listed in Condition 2.2.8. Ensure the screening calculations and tables are correct and present complete species information.

2.11 Apply for an administrative amendment to Pre-Existing Special Permit #42 for Lincoln Memorial Park to remove the funeral home from the boundary.

3. Final plat(s) is/are approved by the City.

Before the approval of a final plat, the public streets, private roadway improvements, sidewalks, public sanitary sewer system, public water system, drainage facilities, land preparation and grading, sediment and erosions control measures, storm water detention/retention facilities, drainageway improvements, street lights, landscaping screens, street trees, temporary turnaround and barricades, and street name signs, must be completed or provisions (bond, escrow or security agreement) to guarantee completion must be approved by the City Law Department. The improvements must be completed in conformance with adopted design standards and within the time period specified in the Land Subdivision Ordinance. A cash contribution to the City in lieu of a bond, escrow, or security agreement may be furnished for street trees on a final plat with 10 or fewer lots.

Permittee agrees:

to complete the installation of sidewalks along the west side of Hazel Scott Drive as shown on the final plat within four (4) years following the approval of the final plat.

to complete the installation or relocation of sidewalks along the east side of South 14th Street and the north side of Pine Lake Road as shown on the final plat within two (2) years following the approval of this final plat.

to construct the sidewalk in the pedestrian way easements in Lot 5.

to complete the public water distribution system to serve this plat within two (2) years following the approval of the final plat.

to complete the public wastewater collection system to serve this plat within two (2) years following the approval of the final plat.

to complete the enclosed public drainage facilities shown on the approved drainage study to serve this plat within two (2) years following the approval of the final plat.

to complete the enclosed private drainage facilities shown on the approved drainage study to serve this plat within two (2) years following the approval of the final plat.

to complete land preparation including storm water detention/retention facilities and open drainageway improvements to serve this plat prior to the installation of utilities and improvements but not more than two (2) years following the approval of the final plat

to complete the planting of the street trees along the west side of Hazel Scott Drive within this plat within two (2) years following the approval of the final plat.

to complete the installation of street trees along the east side of South 14th Street and the north side of Lake Road as shown on the final plat within two (2) years following the approval of this final plat.

to complete the installation of the street name signs within two (2) years following the approval of the final plat.

to complete the installation of the permanent markers prior to construction on or conveyance of any lot in the plat.

to complete any other public or private improvement or facility required by Chapter 26.23 (Development Standards) of the Land Subdivision Ordinance in a timely manner which inadvertently may have been omitted from the above list of required improvements.

to submit to the Director of Public Works a plan showing proposed measures to control sedimentation and erosion and the proposed method to temporarily stabilize all graded land for approval.

to comply with the provisions of the Land Preparation and Grading requirements of the Land Subdivision Ordinance.

to complete the public and private improvements shown on the Use Permit.

to keep taxes and special assessments on the outlots from becoming delinquent.

to maintain the outlots and private improvements in a condition as near as practical to the original construction on a permanent and continuous basis.

to maintain the street trees located on private property and landscape screens on a permanent and continuous basis.

to maintain the sidewalks in the pedestrian way easements on Lot 5 and all of its elements in a condition as near as practical to the original or as constructed condition in order to provide the user with a safe and convenient facility on a permanent and continuous basis.

to maintain and supervise the private facilities which have common use or benefit in a condition as near as practical to the original construction on a permanent and continuous basis, and to recognize that there may be additional maintenance issues

or costs associated with providing for the proper functioning of storm water detention/retention facilities as they were designed and constructed within the development and that these are the responsibility of the land owner.

to retain ownership of and the right of entry to the outlots in order to perform the above-described maintenance of the outlots and private improvements on a permanent and continuous basis. However, Permittee(s) may be relieved and discharged of such maintenance obligations upon creating in writing a permanent and continuous association of property owners who would be responsible for said permanent and continuous maintenance subject to the following conditions:

- (1) Permittee shall not be relieved of Permittee's maintenance obligation for each specific private improvement until a registered professional engineer or nurseryman who supervised the installation of said private improvement has certified to the City that the improvement has been installed in accordance with approved plans.
- (2) The maintenance agreements are incorporated into covenants and restrictions in deeds to the subdivided property and the documents creating the association and the restrictive covenants have been reviewed and approved by the City Attorney and filed of record with the Register of Deeds.

to pay all design, engineering, labor, material, inspection, and other improvement costs.

to relinquish the right of direct vehicular access from lot 1-3 to S. 14th Street and Pine Lake Road, and from Lot 6 to Hazel Scott Drive.

- 3.1 Submit an application to vacate the right-of-way stub on Private Drive "A" with the final plat application.

Standard Conditions:

4. The following conditions are applicable to all requests:
 - 4.1 Before occupying buildings all development and construction is to substantially comply with the approved plans.
 - 4.2 All privately-owned improvements, including landscaping are to be permanently maintained by the Permittee or an appropriately established homeowners association approved by the City.

- 4.3 The physical location of all setbacks and yards, buildings, parking and circulation elements, and similar matters must be in substantial compliance with the location of said items as shown on the approved site plan.
- 4.4 This resolution's terms, conditions, and requirements bind and obligate the permittee, its successors and assigns.
- 4.5 The applicant shall sign and return the letter of acceptance to the City Clerk. This step should be completed within 60 days following the approval of the special permit. The City Clerk shall file a copy of the resolution approving the special permit and the letter of acceptance with the Register of Deeds, filing fees therefore to be paid in advance by the applicant. Building permits will not be issued unless the letter of acceptance has been filed.

Prepared by

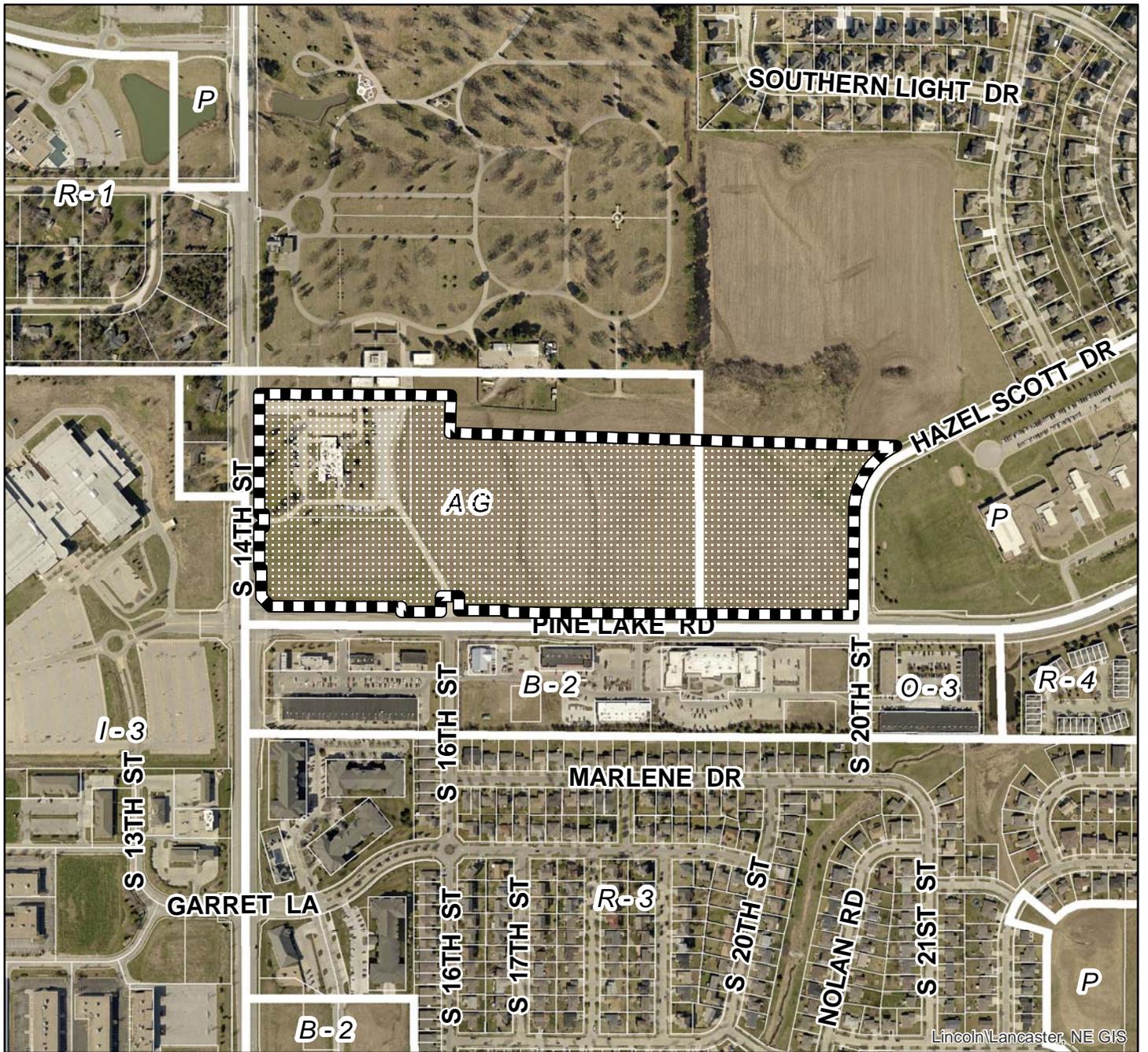
Rachel Jones, Planner

DATE: October 4, 2016

APPLICANT: Costco Wholesale c/o TJ Design Strategies
2311 W. 22nd Street, Suite 208
Oak Brook, Illinois 60523

OWNER: The Lincoln Memorial Park Cemetery Association
1929 Allen Parkway
Houston, Texas 77019

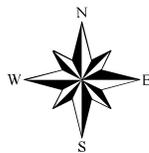
CONTACT: Theodore R. Johnson
2311 W. 22nd Street, Suite 208
Oak Brook, Illinois 60523



Lincoln/Lancaster, NE GIS

2016 aerial

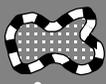
Use Permit #: UP16009
Costco Wholesale
S 14th St & Pine Lake Rd

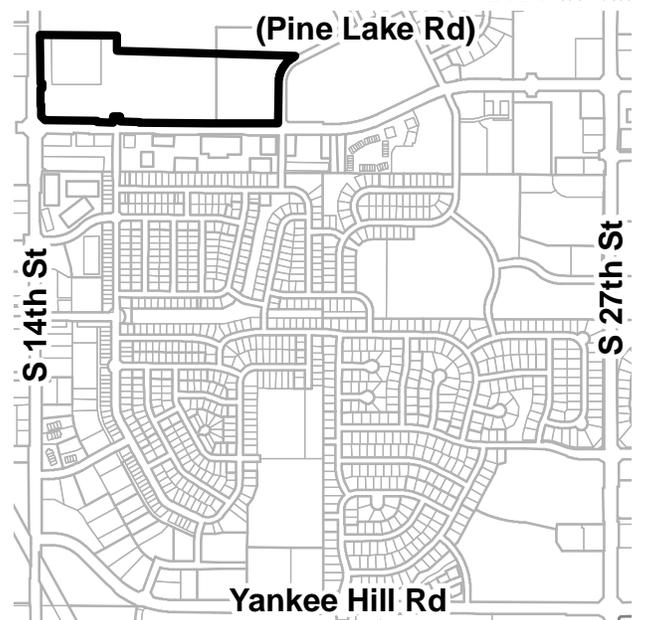


One Square Mile:
Sec.24 T09N R06E

Zoning:

- R-1 to R-8 Residential District
- AG Agricultural District
- AGR Agricultural Residential District
- O-1 Office District
- O-2 Suburban Office District
- O-3 Office Park District
- R-T Residential Transition District
- B-1 Local Business District
- B-2 Planned Neighborhood Business District
- B-3 Commercial District
- B-4 Lincoln Center Business District
- B-5 Planned Regional Business District
- H-1 Interstate Commercial District
- H-2 Highway Business District
- H-3 Highway Commercial District
- H-4 General Commercial District
- I-1 Industrial District
- I-2 Industrial Park District
- I-3 Employment Center District
- P Public Use District

	Area of Application
	Zoning Jurisdiction Lines
	City of Lincoln Jurisdiction



USE PERMIT LEGAL DESCRIPTION

A LEGAL DESCRIPTION FOR A TRACT OF LAND COMPOSED OF A PORTION OF LOT 92 I.T., AND A PORTION OF OUTLOT D, THE RIDGE 13TH ADDITION, LOCATED IN THE NORTHWEST QUARTER OF SECTION 24, TOWNSHIP 9 NORTH, RANGE 6 EAST, OF THE 6TH P.M., CITY OF LINCOLN, LANCASTER COUNTY, NEBRASKA, AND MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID OUTLOT D, THE RIDGE 13TH ADDITION, SAID POINT BEING ON THE NORTH RIGHT-OF-WAY LINE OF PINE LAKE ROAD AND ON THE WEST RIGHT-OF-WAY LINE OF HAZEL SCOTT DRIVE; THENCE, WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID OUTLOT D, AND THE SOUTH LINE OF SAID LOT 92 I.T., ON AN ASSUMED BEARING OF NORTH 89 DEGREES 35 MINUTES 25 SECONDS WEST, A DISTANCE OF 1269.84 FEET TO A POINT; THENCE NORTH 00 DEGREES 24 MINUTES 35 SECONDS EAST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 7.00 FEET TO A POINT; THENCE NORTH 89 DEGREES 35 MINUTES 25 SECONDS WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 175.50 FEET TO A POINT; THENCE NORTH 00 DEGREES 24 MINUTES 35 SECONDS EAST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 50.00 FEET TO A POINT; THENCE NORTH 89 DEGREES 35 MINUTES 25 SECONDS WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 66.00 FEET TO A POINT; THENCE SOUTH 00 DEGREES 24 MINUTES 35 SECONDS WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 57.00 FEET TO A POINT; THENCE NORTH 89 DEGREES 35 MINUTES 25 SECONDS WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 144.59 FEET TO A POINT; THENCE NORTH 00 DEGREES 24 MINUTES 35 SECONDS EAST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 17.00 FEET TO A POINT; THENCE NORTH 89 DEGREES 35 MINUTES 25 SECONDS WEST, ALONG SAID NORTH RIGHT-OF-WAY LINE, SAID LINE BEING THE SOUTH LINE OF SAID LOT 92 I.T., A DISTANCE OF 480.00 FEET TO A POINT ON THE EAST RIGHT-OF-WAY LINE OF SOUTH 14TH STREET; THENCE NORTH 44 DEGREES 50 MINUTES 33 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING A SOUTHWEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 46.88 FEET TO A POINT; THENCE NORTH 00 DEGREES 05 MINUTES 40 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 105.72 FEET TO A POINT; THENCE SOUTH 89 DEGREES 54 MINUTES 20 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 7.00 FEET TO A POINT; THENCE NORTH 00 DEGREES 05 MINUTES 40 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 149.84 FEET TO A POINT; THENCE NORTH 89 DEGREES 54 MINUTES 20 SECONDS EAST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 17.00 FEET TO A POINT; THENCE NORTH 00 DEGREES 05 MINUTES 40 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT

92 I.T., A DISTANCE OF 50.00 FEET TO A POINT; THENCE SOUTH 89 DEGREES 54 MINUTES 20 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 17.00 FEET TO A POINT; THENCE NORTH 00 DEGREES 05 MINUTES 40 SECONDS WEST, ALONG SAID EAST RIGHT-OF-WAY LINE, SAID LINE BEING THE WEST LINE OF SAID LOT 92 I.T., A DISTANCE OF 439.30 FEET TO THE NORTHWEST CORNER OF LOT SAID 92 I.T., SAID POINT BEING THE SOUTHWEST CORNER OF LOT 77 I.T.; THENCE SOUTH 89 DEGREES 32 MINUTES 37 SECONDS EAST, ALONG THE NORTH LINE OF SAID LOT 92 I.T., SAID LINE BEING THE SOUTH LINE OF SAID LOT 77 I.T., A DISTANCE OF 703.30 FEET TO A POINT; THENCE SOUTH 01 DEGREES 40 MINUTES 50 SECONDS WEST, A DISTANCE OF 137.47 FEET TO A POINT; THENCE SOUTH 88 DEGREES 19 MINUTES 10 SECONDS EAST A DISTANCE OF 1635.85 FEET TO A POINT ON THE EAST LINE OF SAID OUTLOT D, SAID POINT BEING ON THE NORTHWEST RIGHT-OF-WAY LINE OF HAZEL SCOTT DRIVE; THENCE SOUTH 63 DEGREES 26 MINUTES 42 SECONDS WEST, ALONG THE EAST LINE OF SAID OUTLOT D, SAID LINE BEING A NORTHWEST RIGHT-OF-WAY LINE OF HAZEL SCOTT DRIVE, A DISTANCE OF 4.77 FEET TO A POINT OF CURVATURE; THENCE ALONG A CURVE TO THE LEFT, HAVING A RADIUS OF 272.37 FEET, A DELTA ANGLE OF 63 DEGREES 03 MINUTES 51 SECONDS, AN ARC LENGTH OF 299.79 FEET, ALONG AN EASTERLY LINE OF SAID OUTLOT D, SAID LINE BEING A NORTHWEST RIGHT-OF-WAY LINE OF SAID HAZEL SCOTT DRIVE, A CHORD BEARING OF SOUTH 31 DEGREES 54 MINUTES 46 SECONDS WEST, AND A CHORD DISTANCE OF 284.89 FEET TO A POINT; THENCE SOUTH 00 DEGREES 22 MINUTES 50 SECONDS WEST, ALONG AN EAST LINE OF SAID OUTLOT D, SAID LINE BEING A WEST RIGHT-OF-WAY LINE OF SAID HAZEL SCOTT DRIVE, A DISTANCE OF 375.41 FEET TO THE POINT OF BEGINNING.

SAID TRACT CONTAINS A CALCULATED AREA OF 1,497,660.94 SQUARE FEET OR 34.38 ACRES, MORE OR LESS.

F:\Projects\015-3421\40-Design\Survey\SRVY\Legal Descriptions and Exhibits\Overall B2 Rezoning Legal.docx

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GENERAL SITE NOTES

1. SANITARY SEWERS & WATER MAINS ARE TO BE PUBLIC UNLESS OTHERWISE NOTED.
2. DIMENSIONS ALONG CURVES ARE CHORD DISTANCES.
3. PAVING RADII TO BE 20' UNLESS OTHERWISE NOTED.
4. INTERSECTION ANGLES SHALL BE 90° ±10' UNLESS OTHERWISE NOTED.
5. ELEVATIONS ARE BASED ON NAVD 1988.
6. SIDEWALKS SHALL BE 4' WIDE MINIMUM AND SIDEWALK EASEMENTS SHALL BE 10' WIDE. (UNLESS OTHERWISE NOTED)
7. AN ASSOCIATION MAY BE ESTABLISHED TO MAINTAIN OPEN SPACES.
8. LOT DIMENSIONS ARE APPROXIMATE AND MAY VARY AT THE TIME OF FINAL PLAT.
9. STREET DIMENSIONS ARE TO BACK OF CURB.
10. EXACT LOCATIONS OF WATER, SEWER, DRAINAGE AND PAVING WILL BE SUBMITTED WITH INDIVIDUAL SITE PLANS AT TIME OF BUILDING PERMIT IN ACCORDANCE WITH CITY OF LINCOLN DESIGN STANDARDS.
11. DETAILS OF ALL SIGNS, INCLUDING TYPE, HEIGHT AND SIZE, WILL BE SUBMITTED SEPARATELY FOR REVIEW WITH THE BUILDING PERMIT AND NEED NOT BE SHOWN ON THE PLAN, AND SHALL BE IN ACCORDANCE WITH LINCOLN MUNICIPAL CODE TITLE 27.
12. THE "CENTER SIGN" FOR THIS USE PERMIT WILL BE LOCATED ON LOT 5 ONLY.
13. BUILDING LAYOUTS SHOWN ARE CONCEPTUAL AND MAY VARY AT TIME OF BUILDING PERMIT.
14. BUILDING SETBACKS SHALL BE PER THE B-2 ZONING DISTRICT.
15. PARKING WILL BE REGULATED ACCORDING TO THE B-2 ZONING ORDINANCE.
16. A DEVIATION REQUEST TO THE ACCESS MANAGEMENT POLICY FOR INTERSECTION SPACING IS REQUESTED FOR THE FOLLOWING INTERSECTIONS:
 - A.) THE SOUTHERN 14TH STREET ACCESS TO THE EXISTING FUNERAL HOME SHALL BECOME A PRIVATE DRIVE THAT ACCESSES LOTS 2,3, AND 4. A RIGHT TURN LANE WILL BE BUILT AND THE SIDEWALK RELOCATED TO BE AT LEAST 9 FEET BEHIND BACK OF CURB.
 - B.) IF THE EXISTING FUNERAL HOME ON LOT 1 REDEVELOPS INTO DIFFERENT COMMERCIAL USES, THE FOLLOWING IS REQUIRED: THE NORTHERN DRIVEWAY WILL BE RECONFIGURED TO BETTER SERVE THE MULTIPLE COMMERCIAL PROPERTIES. A RIGHT HAND TURN LANE WILL BE REQUIRED TO THE NORTHERN DRIVEWAY ON 14TH STREET AND THE SIDEWALK RELOCATED TO PROVIDE ADEQUATE SAFETY FOR PEDESTRIANS. ALL IMPROVEMENTS, AND RIGHT OF WAY DEDICATION, WILL BE AT THE EXPENSE OF THE DEVELOPER REDEVELOPING THE FUNERAL HOME PROPERTY.
 - C.) THE RIGHT IN / RIGHT OUT ON PINE LAKE ROAD (18TH STREET) WILL NOT SERVE MULTIPLE PROPERTIES AS RECOMMENDED IN THE ACCESS MANAGEMENT POLICY. THE RIGHT TURN LANES CONSTRUCTED ON PINE LAKE ROAD SHALL BE DESIGNED TO PROVIDE SAFE PEDESTRIAN CROSSING FOR THE BIKE TRAIL. THE BIKE TRAIL SHALL BE LOCATED A MINIMUM OF 9 FEET BEHIND BACK OF CURB OF THE TURN LANES. 10 FEET OF ADDITIONAL RIGHT OF WAY WILL BE REQUIRED.
17. THE INTERSECTION OF 16TH AND PINE LAKE ROAD SHALL MAINTAIN FULL ACCESS IN ALL DIRECTIONS.
18. OUTLOT "B" SHALL BE RESERVED FOR RIGHT OF WAY FOR A POTENTIAL ROUNDABOUT AT 20TH / HAZEL SCOTT DRIVE AND PINE LAKE ROAD. THE CITY HAS UNTIL MARCH 1ST, 2017 TO FUND THE ROUNDABOUT OPTION. IF THE CITY HAS NOT SECURED FUNDING BY MARCH 1ST, 2017, THE DEVELOPER MAY PROCEED WITH TURN LANE RECONSTRUCTION AND TRAFFIC SIGNAL INSTALLATION AT THEIR COST. OUTLOT "B" MAY BE UTILIZED FOR DEVELOPMENT IF THE TRAFFIC SIGNAL IS INSTALLED. IF THE CITY PROCEEDS WITH THE ROUNDABOUT PROJECT, THE CONSTRUCTION SHALL BE COMPLETED BY OCTOBER, 2017, AHEAD OF COSTCO'S GRAND OPENING.
19. TRAFFIC SIGNAGE SHALL BE INSTALLED TO PROHIBIT DELIVERY'S AND SERVICE VEHICLES FROM TURNING LEFT ONTO HAZEL SCOTT DRIVE.
20. A LANDSCAPE PLAN WILL BE PROVIDED AT TIME OF BUILDING PERMIT. A STREET TREES PLAN FOR PINE LAKE, 14TH STREET, AND HAZEL SCOTT DRIVE WILL BE DESIGNED WITH THE FINAL PLAT.
21. THE COSTCO BUILDING ON LOT 5 (WAREHOUSE & FUELING STATION) SHALL SUBSTANTIALLY CONFORM TO THE BUILDING ELEVATIONS INCLUDED IN THIS USE PERMIT (SHEETS 6 THRU 10).
22. ALL BUILDINGS IN THE CENTER WILL MEET THE FOLLOWING DESIGN STANDARDS:
 - A.) ALL BUILDINGS WILL INCLUDE AT LEAST 20% OF THE VERTICAL SURFACE AS DECORATIVE CMU, BRICK, OR STONE. THIS MATERIAL WILL BE USED ON ANY BUILDING SURFACE THAT FACES A PUBLIC STREET. THE BUILDINGS WILL UTILIZE SIMILAR BUILDING MATERIALS AND COLORS AS THE COSTCO BUILDING.
23. A PROGRAMMED FLASHING SIGNAL SHALL BE INSTALLED AT THE STOP SIGN AT THE RIGHT OUT LOCATION ON PINE LAKE ROAD. THIS SIGNAL WILL BE PROGRAMMED TO OPERATE DURING SCHOOL DISMISSAL TIMES TO ALERT DRIVERS OF POTENTIAL PEDESTRIANS WALKING EASTBOUND DURING THIS TIME PERIOD.
24. A 5' PEDESTRIAN SIDEWALK SHALL BE INSTALLED ON THE WEST SIDE OF 16TH STREET ENTERING THE COSTCO PROPERTY. THIS SIDEWALK SHALL FOLLOW THE DRIVEWAY NORTH AND EAST AROUND THE BUILDING AND SHALL CONNECT TO HAZEL SCOTT DRIVE NORTH OF THE DRIVEWAY CONNECTION. THIS SIDEWALK WILL PROVIDE A PEDESTRIAN OPTION FOR STUDENTS WALKING TO THE NEIGHBORING RESIDENTIAL NEIGHBORHOOD.
25. THE WATER QUALITY IMPROVEMENTS FOR THIS SITE SHALL BE INCORPORATED INTO THE STORMWATER DETENTION CELLS. THE MAINTENANCE AGREEMENT, CALCULATION WORKSHEETS, AND EASEMENT DOCUMENTS SHALL BE SUBMITTED AT TIME OF BUILDING PERMIT.
26. HAZEL SCOTT DRIVE SHALL BE RECONFIGURED AS SHOWN ON THE USE PERMIT SITE PLAN TO DIRECT TRAFFIC PRIMARILY TO THE COMMERCIAL DEVELOPMENT AND AWAY FROM THE NEIGHBORHOOD.

COSTCO CENTER USE PERMIT

LINCOLN, NEBRASKA

PRELIMINARY CIVIL ENGINEERING

OWNER

LINCOLN MEMORIAL PARK CEMETERY
 6800 S. 14TH ST.
 LINCOLN, NE 68512
 PHONE: 402-423-1515

DEVELOPER

COSTCO WHOLESALE
 999 LAKE DRIVE
 ISSAQUAH, WA. 98027
 PHONE: 425-313-8103

ENGINEER & PREPARER

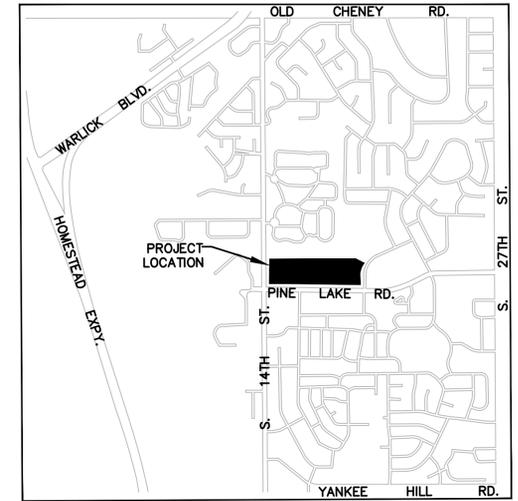
OLSSON ASSOCIATES
 601 "P" STREET
 LINCOLN, NE. 68508
 PHONE: 402-474-6311

SHEET INDEX

1	COVER / NOTE PLAN
2	SITE PLAN
3	SITE PLAN
4	GRADING & DRAINAGE PLAN
5	PROFILE SHEET
6	BUILDING ELEVATIONS
7	BUILDING ELEVATIONS
8	BUILDING ELEVATIONS
9	BUILDING ELEVATIONS
10	BUILDING ELEVATIONS

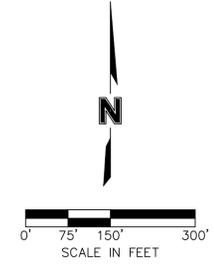
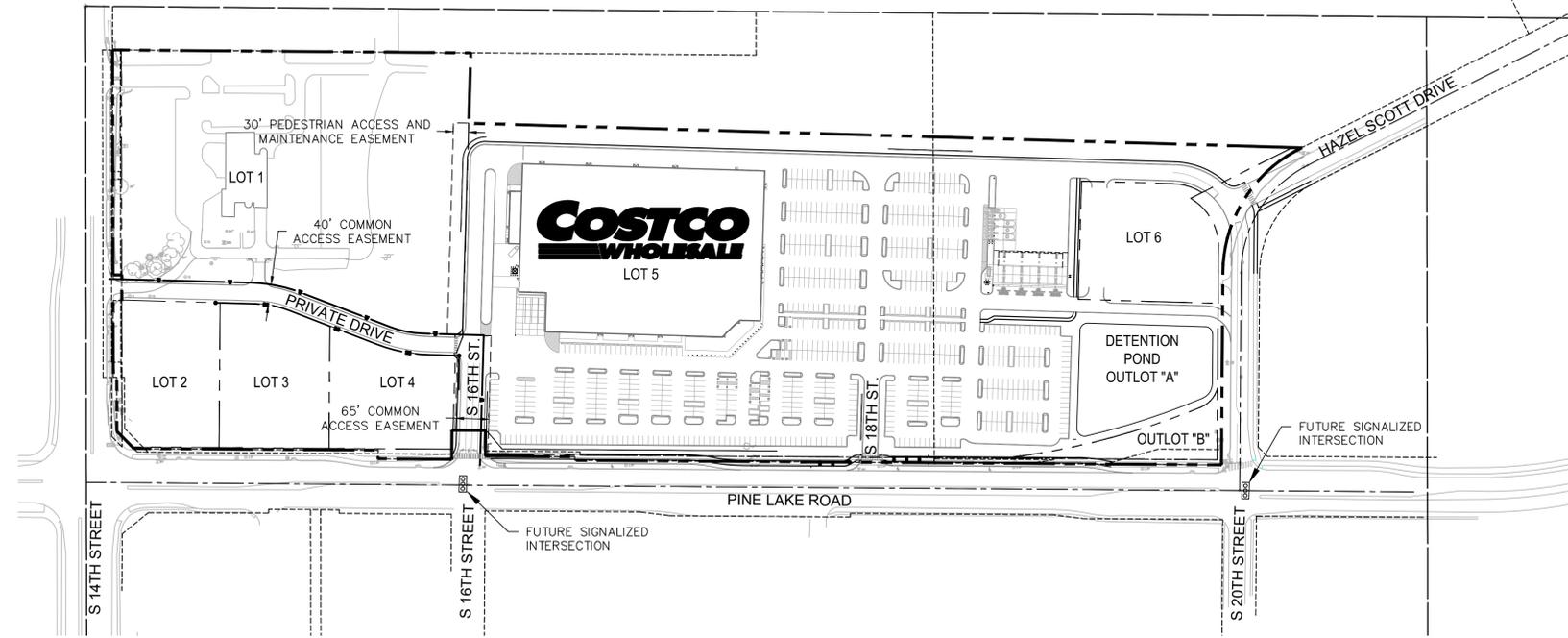
ZONING

THE PROPOSED ZONING FOR THE PROPERTY IS B-2 COMMERCIAL



BLOCK/ LOT NUMBER	USE	FLOOR AREA (FT ²)	REQUIRED PARKING STALLS	PROVIDED PARKING STALLS	HEIGHT
BLOCK 1					
1	COMMERCIAL / FUNERAL HOME				40'
2	COMMERCIAL	5,000	23		40'
3	COMMERCIAL	4,000	18		40'
4	COMMERCIAL	4,000	18		40'
5	DISCOUNT CLUB COMMERCIAL	157,000	707	752	40'
6	COMMERCIAL	7,000	32		40'
TOTAL		177,000	798		
RATIO			4.5 / 1,000 S.F.		

LOT 1 CURRENTLY IS A FUNERAL HOME. AN AMENDMENT TO THE USE PERMIT WILL BE REQUIRED IF THIS LOT REDEVELOPS INTO COMMERCIAL USES.
 LOTS 2,3,4, AND 6 WILL REQUIRE AN ADMINISTRATIVE AMENDMENT TO THE USE PERMIT THAT INCLUDES A SITE PLAN TO VERIFY LAND USE AND PARKING CONFORMANCE.



OLSSON ASSOCIATES
 601 P STREET
 P.O. BOX 9408
 LINCOLN, NE 68508
 TEL: 402.474.6311
 FAX: 402.474.5160



REV. NO.	DATE	REVISIONS DESCRIPTION

COVER / NOTE PLAN
 COSTCO CENTER
 USE PERMIT
 2016
 LINCOLN, NEBRASKA

dr: CAD
 id: KLU
 rev: BHL
 QA/QC: MCP
 no: 015-3421
 dt: 09-14-2016



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4. INTERSECTION ANGLES SHALL BE 90' ±10' UNLESS OTHERWISE NOTED.
5. ELEVATIONS ARE BASED ON NAVD 1988.
6. SIDEWALKS SHALL BE 4' WIDE MINIMUM AND SIDEWALK EASEMENTS SHALL BE 10' WIDE. (UNLESS OTHERWISE NOTED)
7. AN ASSOCIATION MAY BE ESTABLISHED TO MAINTAIN OPEN SPACES.
8. LOT DIMENSIONS ARE APPROXIMATE AND MAY VARY AT THE TIME OF FINAL PLAT.
9. STREET DIMENSIONS ARE TO BACK OF CURB.
10. EXACT LOCATIONS OF WATER, SEWER, DRAINAGE AND PAVING WILL BE SUBMITTED WITH INDIVIDUAL SITE PLANS AT TIME OF BUILDING PERMIT IN ACCORDANCE WITH CITY OF LINCOLN DESIGN STANDARDS.
11. DETAILS OF ALL SIGNS, INCLUDING TYPE, HEIGHT AND SIZE, WILL BE SUBMITTED SEPARATELY FOR REVIEW WITH THE BUILDING PERMIT AND NEED NOT BE SHOWN ON THE PLAN, AND SHALL BE IN ACCORDANCE WITH LINCOLN MUNICIPAL CODE TITLE 27.
12. THE "CENTER SIGN" FOR THIS USE PERMIT WILL BE LOCATED ON LOT 5 ONLY.
13. BUILDING SETBACKS SHALL BE PER THE B-2 ZONING DISTRICT.
14. PARKING WILL BE REGULATED ACCORDING TO CHAPTER 27.67 OF THE ZONING ORDINANCE.
15. A DEVIATION TO THE ACCESS MANAGEMENT POLICY FOR THE INTERSECTIONS AS NOTED ON THE PLANS WILL BE REQUESTED AT TIME OF CONSTRUCTION DESIGN. THE LENGTH OF TAPER, STORAGE LENGTH, AND DECELERATION LENGTH FOR RIGHT TURN LANES ARE DEVIATIONS TO THE ACCESS MANAGEMENT POLICY AND ARE REQUIRED DUE TO EXISTING GEOMETRIC CONSTRAINTS. THE LENGTH OF RIGHT TURN LANES ARE ALSO REDUCED AS RIGHT TURNS ARE A "FREE FLOWING" TRAFFIC MOVEMENT.
 - A.) THE SOUTHERN 14TH STREET ACCESS TO THE EXISTING FUNERAL HOME SHALL BECOME A PRIVATE DRIVE THAT ACCESSES LOTS 2,3, AND 4. A RIGHT TURN LANE WILL BE BUILT AND THE SIDEWALK RELOCATED TO BE AT LEAST 9 FEET BEHIND BACK OF CURB.
 - B.) IF THE EXISTING FUNERAL HOME ON LOT 1 REDEVELOPS INTO DIFFERENT COMMERCIAL USES, THE FOLLOWING IS REQUIRED: THE NORTHERN DRIVEWAY WILL BE RECONFIGURED TO BETTER SERVE THE MULTIPLE COMMERCIAL PROPERTIES. A RIGHT TURN LANE WILL BE REQUIRED FOR THE NORTHERN DRIVEWAY ON 14TH STREET AND THE SIDEWALK RELOCATED TO PROVIDE ADEQUATE SAFETY FOR PEDESTRIANS. ALL IMPROVEMENTS, AND RIGHT OF WAY DEDICATION, WILL BE AT THE EXPENSE OF THE DEVELOPER REDEVELOPING THE FUNERAL HOME PROPERTY. IN ADDITION, A NORTHBOUND RIGHT TURN LANE SHALL BE REQUIRED TO BE INSTALLED AT THE TIME FOR THE NORTH ACCESS POINT ON S 14TH STREET.
 - C.) THE RIGHT IN / RIGHT OUT ON PINE LAKE ROAD (18TH STREET) IS A DEVIATION AS IT DOES NOT SERVE MULTIPLE LOTS AND DOES NOT MEET THE 660' INTERSECTION SPACING REQUIREMENTS FOR A MINOR ARTERIAL.
16. THE RIGHT TURN LANES CONSTRUCTED ON PINE LAKE ROAD SHALL BE DESIGNED TO PROVIDE SAFE PEDESTRIAN CROSSING FOR THE BIKE TRAIL. THE BIKE TRAIL SHALL BE LOCATED A MINIMUM OF 9 FEET BEHIND BACK OF CURB OF THE TURN LANES. 10 FEET OF ADDITIONAL RIGHT OF WAY, OR EASEMENT WILL BE PROVIDED. THE BIKE TRAIL SHALL BE LOCATED A MINIMUM OF 10 FEET BEHIND BACK OF CURB TO PROVIDE ADEQUATE SEPARATION AND ROOM FOR STREET TREES. STREET TREES SHALL BE INSTALLED 6 FEET BEHIND BACK OF CURB.

17. A LANDSCAPE PLAN WILL BE PROVIDED AT TIME OF BUILDING PERMIT. A STREET TREES PLAN FOR PINE LAKE, 14TH STREET, AND HAZEL SCOTT DRIVE WILL BE SUBMITTED WITH THE FINAL PLAT.
18. THE BUILDINGS ON LOT 5 SHALL SUBSTANTIALLY CONFORM TO THE BUILDING ELEVATIONS INCLUDED IN THIS USE PERMIT (SHEETS 6 THRU 10).
19. ALL BUILDINGS IN THE CENTER, INCLUDING THE COSTCO BUILDING SHALL MEET THE FOLLOWING DESIGN STANDARDS:
 - A.) BUILDINGS OF 50,000 SQUARE FEET OR GREATER WILL INCLUDE AT LEAST 20% OF THE GROSS AREA OF VERTICAL SURFACES AS DECORATIVE CMU, BRICK, OR STONE. BUILDINGS OF LESS THAN 50,000 SQUARE FEET WILL INCLUDE AT LEAST 50% OF THE GROSS AREA OF VERTICAL SURFACES AS DECORATIVE CMU, BRICK OR STONE, 30% OF THOSE REQUIRED MASONRY MATERIALS SHALL BE BRICK OR STONE, THE REMAINDER AND ANY MASONRY AREAS IN EXCESS OF 50% MAY BE DECORATIVE CMU. THE REQUIRED MASONRY MATERIALS SHALL BE USED TO THE EXTENT FEASIBLE ON FACADES ORIENTED TOWARDS PUBLIC STREETS, WITH ANY REMAINING REQUIRED MASONRY USED ON OTHER FACADES
 - B.) ALL BUILDINGS IN THE CENTER WILL UTILIZE BUILDING MATERIALS AND COLORS COMPATIBLE TO THE COSTCO BUILDING.
 - C.) CALCULATIONS OF THE GROSS AREA OF VERTICAL SURFACES, THE REQUIRED AREA OF MASONRY ON EACH FACADE SHALL BE PROVIDED AT THE TIME OF APPLICATION FOR BUILDING PERMITS.
20. A 5' PEDESTRIAN SIDEWALK SHALL BE INSTALLED ON THE WEST SIDE OF 16TH STREET ENTERING THE COSTCO PROPERTY. THIS SIDEWALK SHALL FOLLOW THE DRIVEWAY NORTH AND EAST AROUND THE BUILDING AND SHALL CONNECT TO HAZEL SCOTT DRIVE NORTH OF THE DRIVEWAY CONNECTION. THIS SIDEWALK WILL PROVIDE A PEDESTRIAN OPTION FOR STUDENTS WALKING TO THE NEIGHBORING RESIDENTIAL NEIGHBORHOOD. THE SIDEWALK SHALL BE CONNECTED TO THE EXISTING SIDEWALK ALONG HAZEL SCOTT DRIVE, SHALL BE PRIVATELY OWNED AND MAINTAINED, AND WILL BE LOCATED IN A PEDESTRIAN WAY EASEMENT. WITH CONSTRUCTION ON LOT 5, A SIDEWALK SHALL BE BUILT ON THE NORTH SIDE OF HAZEL SCOTT DRIVE FROM LOT 5 TO SHADOW RIDGE ROAD.
21. THE WATER QUALITY IMPROVEMENTS FOR THIS SITE SHALL BE INCORPORATED INTO THE STORMWATER DETENTION CELLS. THE MAINTENANCE AGREEMENT, CALCULATION WORKSHEETS, AND EASEMENT DOCUMENTS SHALL BE SUBMITTED AT TIME OF BUILDING PERMIT.

LAND USE TABLE				
BLOCK/ LOT NUMBER	USE	FLOOR AREA (FT2)	REQUIRED PARKING STALLS	PROVIDED PARKING STALLS
BLOCK 1				
1	FUNERAL HOME *	22,000	73	--
2	COMMERCIAL	5,000	17	--
3	COMMERCIAL	4,000	14	--
4	COMMERCIAL	4,000	14	--
5	DISCOUNT CLUB COMMERCIAL	157,000	524	754
6	COMMERCIAL ***	7,000	24	--
TOTAL		199,000	666	
RATIO			1 / 300 S.F.	

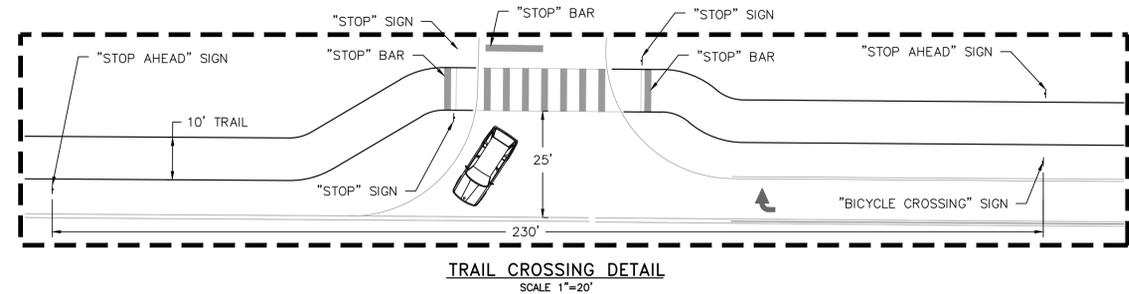
* LOT 1 CURRENTLY IS A FUNERAL HOME. AN AMENDMENT TO THE USE PERMIT WILL BE REQUIRED IF THIS LOT REDEVELOPS INTO COMMERCIAL USES.

** LOTS 2,3,4, AND 6 WILL REQUIRE AN AMENDMENT TO THE USE PERMIT THAT INCLUDES A SITE PLAN TO VERIFY LAND USE AND PARKING CONFORMANCE.

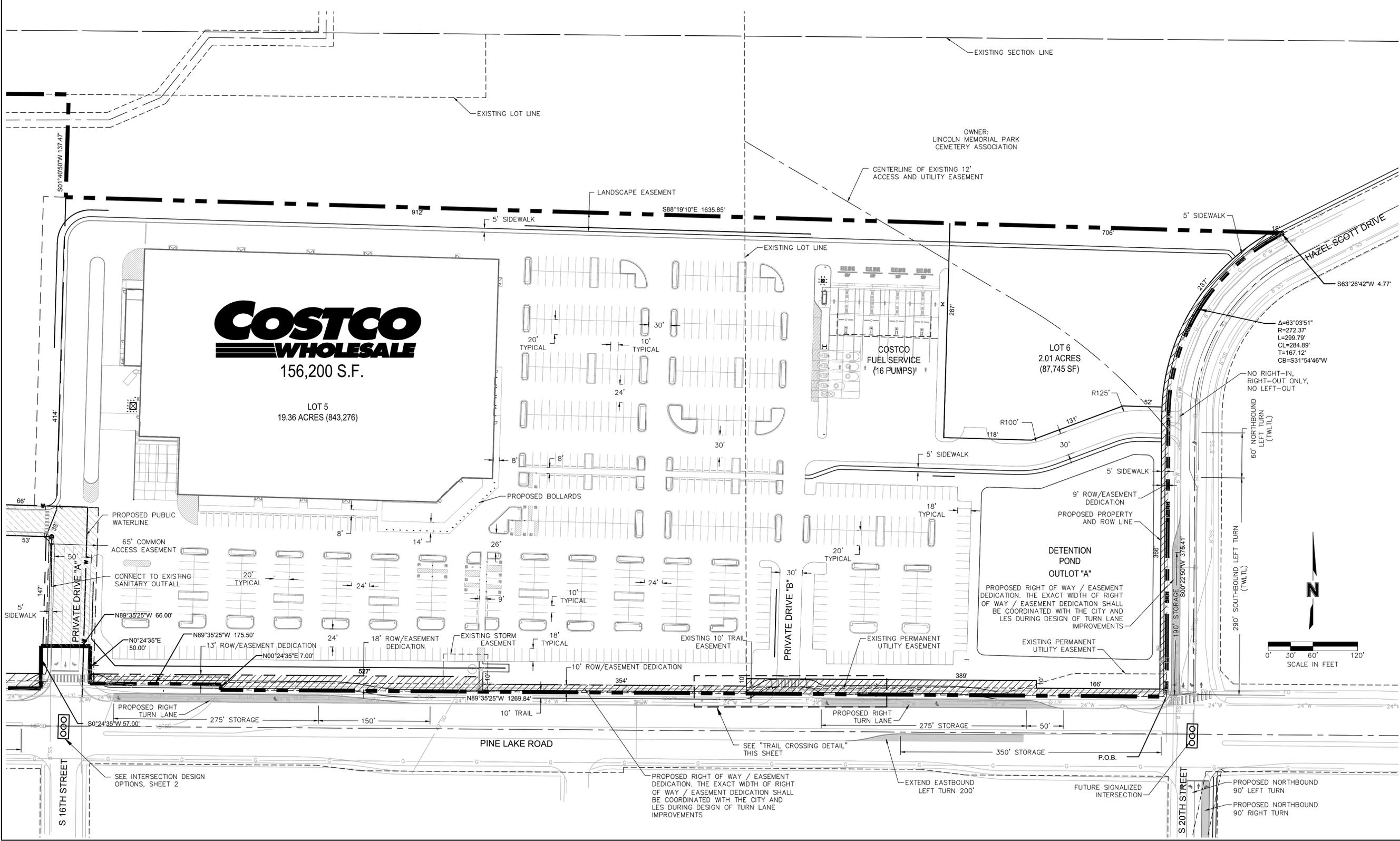
***THE FOLLOWING USES ARE PROHIBITED ON LOT 6: FAST FOOD DRIVE THRU FACILITIES (i.e. MCDONALDS, BURGER KING, WENDYS, AND SIMILAR BUSINESSES), MOTORIZED VEHICLE FUEL SALES FACILITIES, MOTORIZED VEHICLE REPAIR / SERVICE, MOTORIZED VEHICLE WASH FACILITIES, AND THE SALE OF ALCOHOLIC BEVERAGES FOR CONSUMPTION EITHER ON OR OFF THE PREMISES EXCEPT ON-SALE FOR RESTAURANTS.

LEGEND

-  TURN LANE IMPROVEMENTS
-  PROPOSED ACCESS EASEMENT
-  R.O.W. TO BE DEDICATED
-  R.O.W. TO BE DEDICATED FOR INTERSECTION IMPROVEMENTS



DWG: \\fs1\Projects\Projects\015-3421\40-Design\AutoCAD\Preliminary Plans\Sheets\DWG\Civil\Prelim\015-3421.dwg USER: jcs
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 16TH STREET INTERSECTION
 S16TH STREET INTERSECTION
 S20TH STREET INTERSECTION



REV. NO.	DATE	REVISIONS DESCRIPTION

SITE PLAN
COSTCO CENTER
USE PERMIT
 LINCOLN, NEBRASKA
 2016
 REVISIONS

dr:0103	CAD
dr:0103	KLU
dr:0103	BHL
dr:0103	MCP
dr:0103	015-3421
dr:0103	09-14-2016

LEGEND

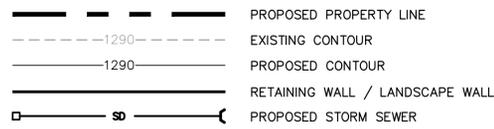


Table 1. – Post-Development Storage Area

Name	Description	Outfall Type	Bottom Elevation (feet)	Top of Berm Surface Area (acre)	Top of Berm Elevation (feet)
East Pond	Post-development	1 – 15" RCP*	1212.2	1.10	1221.0
West Offsite	Hypothetical Post-development	1 – 18" RCP**	1208.0	0.25	1214.4

*Inlet is restricted by 5" orifice at invert 1212.2 feet and stair-stepped, sharp-crested weirs with crests at 1215.7 (1.2 feet long), 1217.0 (5 feet long), and 1218.2 feet (10 feet long).
 **Inlet will need to be restricted by 2" water quality orifice and sharp crested weir with crest at 1211.0 (20 feet long).

Table 2. – Peak Flows from Subbasins and Nodes (cfs)

Location	2-Year	10-Year	100-Year
Exist West Offsite	14	24	36
Proposed West Offsite	17	26	38
Exist West	3.5	6.0	9.1
Prop West	1.1	1.7	2.5
Exist Middle	37	63	97
Middle Offsite	22	38	58
Prop Middle	11	18	25
Middle Combined	23	40	61
Exist East	64	111	169
Prop East	69	108	156
East Offsite	56	97	148
East Combined	62	110	162

Table 3. – Detention Pond Routing – East Pond

Event	Peak Inflow (cfs)	Peak Outflow (cfs)	Peak Elevation (ft)	Peak Volume (ac-ft)
2-Year	69	6.1	1216.90	1.99
10-Year	108	13	1218.29	3.20
100-Year	156	15	1219.96	4.94

Table 4. – Detention Pond Routing – West Offsite

Event	Peak Inflow (cfs)	Peak Outflow (cfs)	Peak Elevation (ft)	Peak Volume (ac-ft)
2-Year	17	12	1211.51	0.32
10-Year	26	14	1212.33	0.45
100-Year	38	16	1213.37	0.64

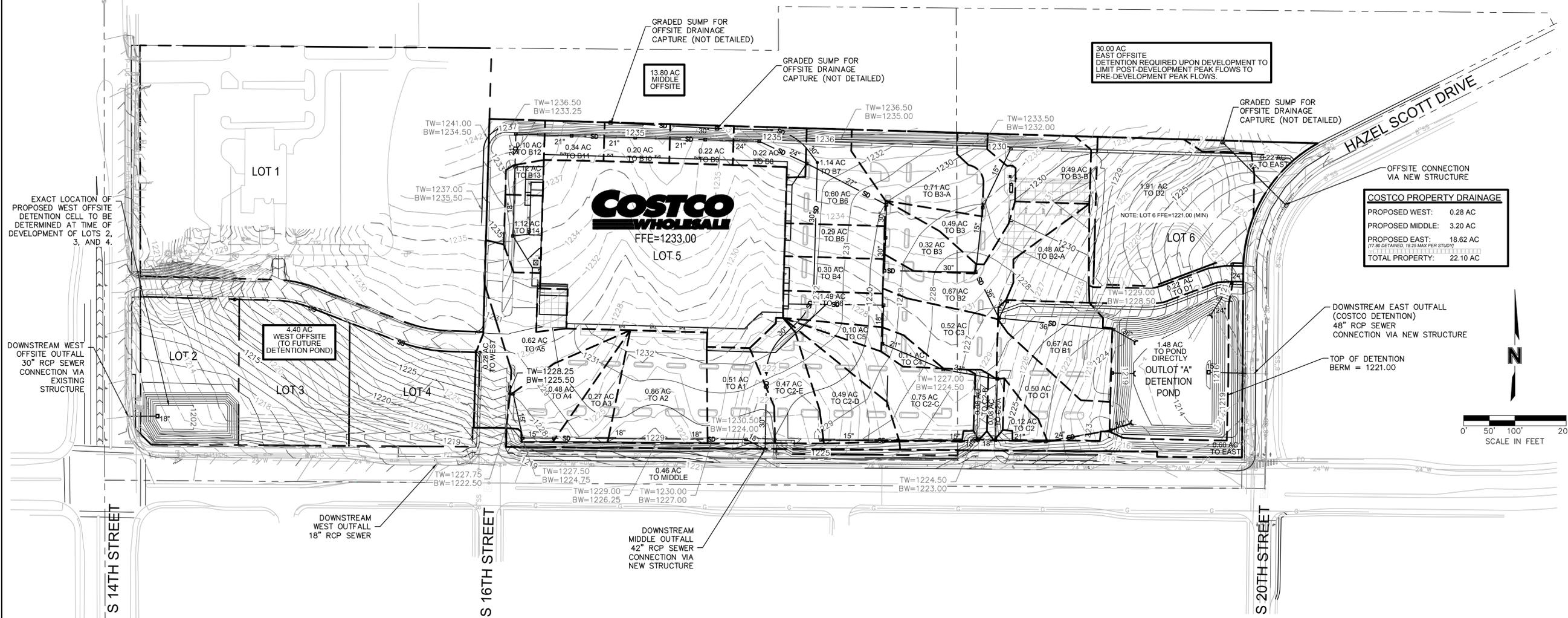
Table 5. – Subbasin Hydrologic Parameters

Location	Drainage Area (ac)	Time of Concentration (min)	Curve Number
Exist West Offsite	4.4	8.0	89
Prop West Offsite	4.4	8.0	95
Exist West	1.11	8.0	89
Prop West	0.28	8.0	98
Exist Middle	25.8	40.1	89
Middle Offsite	13.8	34.2	89
Prop Middle	2.83	8.0	98
Exist East	38.4	31.5	89
Prop East	18.3	8.7	95
East Offsite	30.0	26.2	89

STORM SEWER PIPE AND STRUCTURE TABLE

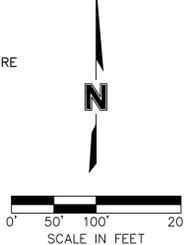
STRUCTURES	FROM	TO	DIRECT AREA (ACRES)	TOTAL AREA (ACRES)	SAFETY FACTOR (NA)	FLOW TIME (MIN)	INTENSITY EST (IN/HR)	Q DESIGN (CFD)	PIPE DESIGN	PIPE LENGTH (L.F.)	PIPE SLOPE (%)	PIPE DIA (IN)	Q FULL CAPACITY (CFD)	Comments	
A5	A4	0.62	0.80	0.80	0.0	0.32	0.00	2.98	18" RCP	80.0	0.75	15	5.61		
A4	A3	0.48	0.80	0.80	0.0	0.32	0.00	2.30	18" RCP	61.0	1.00	15	6.48		
A3	A2	0.27	1.10	0.80	0.3	0.17	0.00	5.28	18" RCP	200.0	1.00	18	10.53		
A2	A1	0.86	1.37	0.80	0.5	0.53	0.00	6.58	18" RCP	175.0	1.25	18	11.78		
A1	MIDDLE	0.51	2.23	0.80	0.0	0.39	0.00	4.13	18" RCP	42.0	1.75	18	13.83	To "Middle" (Excludes ROW)	
B14	B13	1.12	0.80	0.80	0.0	0.42	0.00	5.38	18" RCP	136.0	0.75	18	9.12	Includes Truck Book and 15% of Building Roof	
B13	B12	1.12	2.24	0.80	0.0	0.19	0.00	10.75	21" RCP	62.0	0.50	21	11.23	Includes 25% of Building Roof	
B12	B11	0.10	2.34	0.80	0.0	0.39	0.00	11.23	21" RCP	108.0	0.50	21	11.23		
B11	B10	0.34	2.68	0.80	0.0	0.32	0.00	13.63	21" RCP	124.0	0.75	21	13.76		
B10	B9	0.20	2.88	0.80	0.0	0.31	0.00	13.82	21" RCP	139.0	1.00	21	15.89		
B9	B8	0.22	3.10	0.80	0.0	0.30	0.00	14.68	24" RCP	105.0	0.50	24	16.04		
B8	B7	0.22	3.32	0.80	0.0	0.29	0.00	15.94	24" RCP	102.0	0.50	24	16.04		
B7	B6	1.14	4.46	0.80	0.0	0.46	0.00	23.41	27" RCP	175.0	0.50	27	21.38	Includes 25% of Building Roof	
B6	B5	0.60	5.06	0.80	0.0	0.16	0.00	24.29	30" RCP	64.0	0.50	30	29.08		
B5	B4	0.29	5.35	0.80	0.0	0.16	0.00	25.68	30" RCP	64.0	0.50	30	29.08		
B4	B3	0.30	5.65	0.80	0.0	0.16	0.00	27.12	30" RCP	190.0	0.50	30	29.08		
B3B	B3A	0.49	6.14	0.80	0.0	0.44	0.00	2.85	15" RCP	128.0	1.00	15	6.48		
B3A	B3	0.71	1.20	0.80	0.0	0.35	0.00	5.76	15" RCP	128.0	1.00	15	6.48		
B3	B2	0.32	1.20	0.80	0.0	0.16	0.00	13.44	30" RCP	100.0	0.50	30	47.29		
B2A	B2	0.48	1.48	0.80	0.0	0.16	0.00	2.30	36" Station Inlet	36.0	0.50	15	4.58		
B2	B1	0.67	1.82	0.80	0.0	0.50	0.00	3.22	36" Station Inlet	234.0	0.50	36	47.29		
B1	B0	0.67	8.89	0.80	0.0	0.11	0.00	43.15	36" RCP	48.0	0.50	36	47.29	To East Detention (Costco) Excludes ROW	
C6	C5	1.48	1.48	0.80	0.0	0.18	0.00	7.15	30" RCP	75.0	1.25	18	11.78	Includes 35% of Building Roof	
C5	C4	0.10	1.59	0.80	0.0	0.44	0.00	0.48	30" RCP	188.0	1.25	21	17.76		
C4	C3	0.11	1.70	0.80	0.0	0.30	0.00	0.63	30" RCP	138.0	1.50	21	19.46		
C3	C2	0.62	2.22	0.80	0.0	0.29	0.00	2.50	30" RCP	123.0	1.00	21	15.89		
C2E	C2D	0.47	0.47	0.80	0.0	0.52	0.00	10.66	21" RCP	150.0	1.00	21	6.48		
C2D	C2C	0.49	0.96	0.80	0.0	0.36	0.00	4.61	15" RCP	123.0	1.00	15	6.48		
C2C	C2B	0.75	1.71	0.80	0.0	0.08	0.00	8.21	18" RCP	33.0	1.00	18	10.53		
C2B	C2A	0.98	1.79	0.80	0.0	0.08	0.00	8.69	18" RCP	34.0	1.25	18	11.78		
C2A	C2	0.98	1.87	0.80	0.0	0.24	0.00	0.38	30" RCP	98.0	1.00	18	10.53		
C2	C1	0.12	4.21	0.80	0.0	0.26	0.00	20.21	24" RCP	129.0	1.00	24	22.48		
C1	C0	0.60	4.71	0.80	0.0	0.10	0.00	22.61	30" RCP	41.0	0.50	30	20.08	To East Detention (Costco) Excludes ROW	
D2	D1	1.91	1.91	0.80	0.0	0.17	0.00	9.17	18" RCP	75.0	1.25	18	11.78	Future Outfall Connection	
D1	D0	0.22	2.13	0.80	0.0	0.42	0.00	10.22	18" RCP	188.0	1.25	18	11.78		
P5	P4	13.80	0.46	0.46	34.2	0.37	0.00	38.09	Area Inlet	208.0	1.00	30	41.13		
P4	P3	0.00	13.80	0.46	0.46	0.35	0.00	38.09	Area Inlet	201.0	1.00	30	41.13		
P3	P2	0.00	13.80	0.46	0.46	0.42	0.00	38.09	Junction Manhole	239.0	1.00	30	41.13		
P2	P1	0.00	13.80	0.46	0.46	0.34	0.00	38.09	Junction Manhole	200.0	1.00	30	41.13		
P1	MIDDLE	0.00	13.80	0.46	0.46	0.35	0.00	38.09	30" RCP	200.0	1.00	30	41.13	Offsite To "Middle" (Via Public Line)	
PB	PA	30.00	0.54	0.54	26.2	0.24	0.00	87.20	Area Inlet	42" RCP	170.0	1.00	42	100.88	Offsite To "East" (Via Public Line)

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COSTCO PROPERTY DRAINAGE

PROPOSED WEST:	0.28 AC
PROPOSED MIDDLE:	3.20 AC
PROPOSED EAST:	18.62 AC
<small>(17.80 DETAINED, 0.82 MAX PER STUDY)</small>	
TOTAL PROPERTY:	22.10 AC



1611 P St., Suite 100
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TEL: 402.474.8311
P.O. Box 94608
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REVISIONS DESCRIPTION

REV. NO.	DATE	DESCRIPTION

GRADING & DRAINAGE PLAN

COSTCO CENTER USE PERMIT

REVISIONS

LINCOLN, NEBRASKA

2016

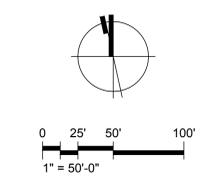
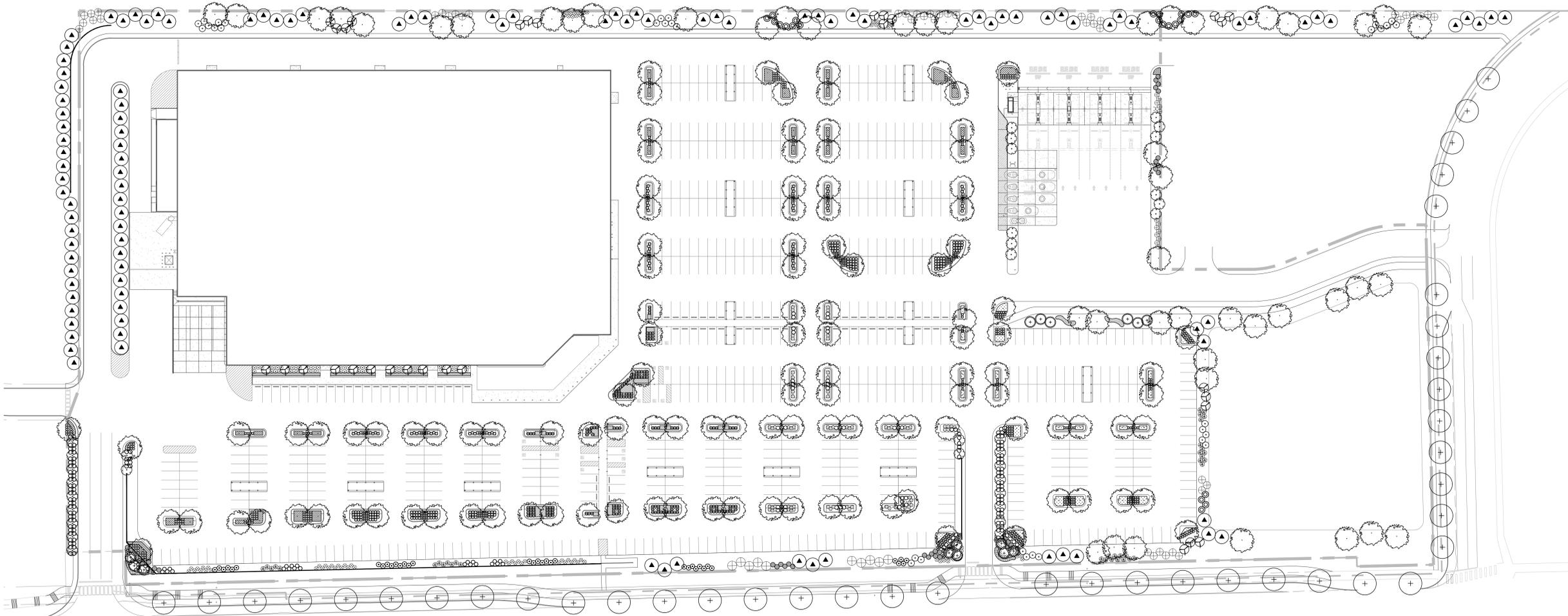
SHEET

4 OF 10

BOTANICAL/COMMON NAMES	INSTALLED SIZE	FORM	SPACING	HT/SPREAD AT MATURITY
CANOPY TREES (TOTAL 167)				
ACER X FREEMANII 'AUTUMN BLAZE' / AUTUMN BLAZE RED MAPLE	3" CAL	B&B	AS SHOWN	50 X 40'
ACER RUBRUM 'RED SUNSET' / RED SUNSET RED MAPLE	3" CAL	B&B	AS SHOWN	45 X 35'
BETULA NIGRA 'HERITAGE' / HERITAGE RIVER BIRCH	12" HT	B&B	AS SHOWN	40 X 30'
CELTIS OCCIDENTALIS / SHACKBERRY	3" CAL	B&B	AS SHOWN	45 X 35'
GLEDTISIA TRIACANTHOS VAR. INERMIS 'SHADEMASTER' / SHADEMASTER HONEYLOCUST	3" CAL	B&B	AS SHOWN	45 X 35'
GYMOCLADUS DIOICUS / KENTUCKY COFFEETREE	3" CAL	B&B	AS SHOWN	50 X 35'
QUERCUS IMBRICARIA / SINGLE OAK	3" CAL	B&B	AS SHOWN	50 X 40'
QUERCUS RUBRA / RED OAK	3" CAL	B&B	AS SHOWN	50 X 45'
TILIA AMERICANA 'REDMOND' / REDMOND LINDEN	3" CAL	B&B	AS SHOWN	35 X 25'
ULMUS X 'MORTON PLAINSMAN' / VANGUARD ELM	3" CAL	B&B	AS SHOWN	45 X 40'
SYRINGA RETICULATA / JAPANESE TREE LILAC	2.5" CAL	B&B	AS SHOWN	25 X 15-25'
ORNAMENTAL TREES (TOTAL 79)				
AESCULUS X CARNEA 'BRIOTII' / RUBY RED FLOWERING CHESTNUT	2" CAL	B&B	AS SHOWN	30-40 X 20-30'
AMELANCHIER X GRANDIFLORA 'AUTUMN BRILLIANCE' / AUTUMN BRILLIANCE SERVICEBERRY	6" HT.	B&B	AS SHOWN	20-25 X 15'
MALUS CV / CRABAPPLE CULTIVARS	2" CAL	B&B	AS SHOWN	10-15 X 12-20'
EVERGREEN TREES (TOTAL 173)				
JUNIPERUS VIRGINIANA 'BURKII' / BURK'S JUNIPER	8-10' HT.	B&B	AS SHOWN	12-15 X 5-6'
PICEA ABIES / NORWAY SPRUCE	8-10' HT.	B&B	AS SHOWN	60-70 X 30'
PICEA GLAUCA VAR. DENSATA / BLACK HILLS SPRUCE	8-10' HT.	B&B	AS SHOWN	30-40 X 15'
PICEA PUNGENS / COLORADO SPRUCE	8-10' HT.	B&B	AS SHOWN	50-60 X 25'
PINUS STROBUS / WHITE PINE	8-10' HT.	B&B	AS SHOWN	70 X 20-35'
THUJA PLICATUM 'GREEN GIANT' / GREEN GIANT ARBORVITAE	8-10' HT.	B&B	AS SHOWN	25-30 X 8-10'
SHRUBS (TOTAL 947)				
HYDRANGEA PANICULATA 'QUICKFIRE' / QUICKFIRE HYDRANGEA	36" HT.	CONT.	7' O.C.	6-8 X 6-8'
ILEX VERTICILLATA 'WINTER RED' / WINTER RED WINTERBERRY AND MALE CULTIVAR	30" HT.	CONT.	6' O.C.	6-9 X 6-8'
JUNIPERUS CHINENSIS 'SEA GREEN' / SEA GREEN JUNIPER	24" SPD.	CONT.	4' O.C.	4' X 4-5'
PHYSOCARPUS OPULIFOLIUS 'SUMMER WINE' / SUMMER WINE NINEBARK	30" HT.	CONT.	5' O.C.	5-7 X 5-7'
PINUS MUGO 'AUREA' / GOLDEN MUGHO PINE	36" SPD.	B&B	5' O.C.	4-6' X 4-6'
RHUS AROMATICA 'GRO-LOW' / GRO-LOW SUMAC	24" SPD.	CONT.	3.5' O.C.	30" X 6'
ROSA 'KNOCKOUT' / KNOCKOUT ROSE	24" SPD.	CONT.	4' O.C.	3-4 X 3-4'
ROSA 'CORAL DRIFT' / CORAL DRIFT ROSE	18" SPD.	CONT.	3' O.C.	1.5 - 3'
SPIRAEA JAPONICA 'MAGIC CARPET' / MAGIC CARPET SPIREA	24" SPD.	CONT.	2' O.C.	18" X 2'
TAXUS X MEDIA 'EVERLOW' / EVERLOW YEW	24" SPD.	CONT.	4' O.C.	3 X 5'
VIBURNUM PLICATUM 'MARISSII' / MARISSII	24" HT.	CONT.	7' O.C.	6-8 X 6-8'
PROPOSED STREET TREE:				
QUERCUS MUEHLBERGII / CHINKAPIN OAK				
QUERCUS RUBRA / RED OAK				
ACER X FREEMANII 'AUTUMN BLAZE' / AUTUMN BLAZE MAPLE				

BOTANICAL/COMMON NAMES	INSTALLED SIZE	FORM	SPACING	HT/SPREAD AT MATURITY
PERENNIALS & ORNAMENTAL GRASSES (749 GRASSES)				
ACHILLEA 'SUNNY SEDUCTION' / SUNNY SEDUCTION YARROW	No.1	CONT.		18" x 18"
ANDROPOGON GERARDII / BIG BLUESTEM				24" O.C.
CALAMAGROSTIS X 'KARL FOERSTER' / KARL FOERSTER FEATHER REED GRASS				24" O.C.
HEMEROCALLIS 'HAPPY RETURNS' / HAPPY RETURNS DAYLILY				18" O.C.
HEMEROCALLIS 'RUBY STELLA' / RUBY STELLA DAYLILY				18" O.C.
MISCANTHUS SINENSIS 'LITTLE KITTEN' / LITTLE KITTEN MAIDENGRASS				24" O.C.
NEPETA X FRAASSENII 'WALKER'S LOW' / WALKER'S LOW CATMINT				24" O.C.
PANICUM VIRGATUM 'SHENANDOAH' / SHENANDOAH SWITCHGRASS				30" O.C.
SORGHASTRUM NUTANS / INDIAN GRASS				24" O.C.
RUDBECKIA FULGIDA 'GOLDSTURM' / GOLDSTURM BLACK-EYED SUSAN				18" O.C.
NOTES:				
1. ALL AREAS NOT SHOWING LANDSCAPE BEDS SHALL RECEIVE SOD UNLESS OTHERWISE NOTED.				
2. ALL SHRUB BEDS/GROUPINGS AND TREE SAUCERS SHALL RECEIVE 2" COMPOSTED SHREDED HARDWOOD BARK MULCH.				
3. ALL BEDS SHALL BE GRADED TO DRAIN; PARKING LOT ISLANDS SHALL BE MOUNDED.				
4. ALL PLANTS SHALL CONFORM TO ANSI Z.60.1 STANDARDS.				
5. ALL LANDSCAPE SHALL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM.				
6. ALL LANDSCAPE MATERIAL SHALL BE MAINTAINED IN GOOD CONDITION BY PEOPLE SPECIALIZING IN SUCH WORK.				
BUILDING FOOTPRINT: 156,508 SF				
PAVED AREAS: 502,105 SF				
ZONING: B2				

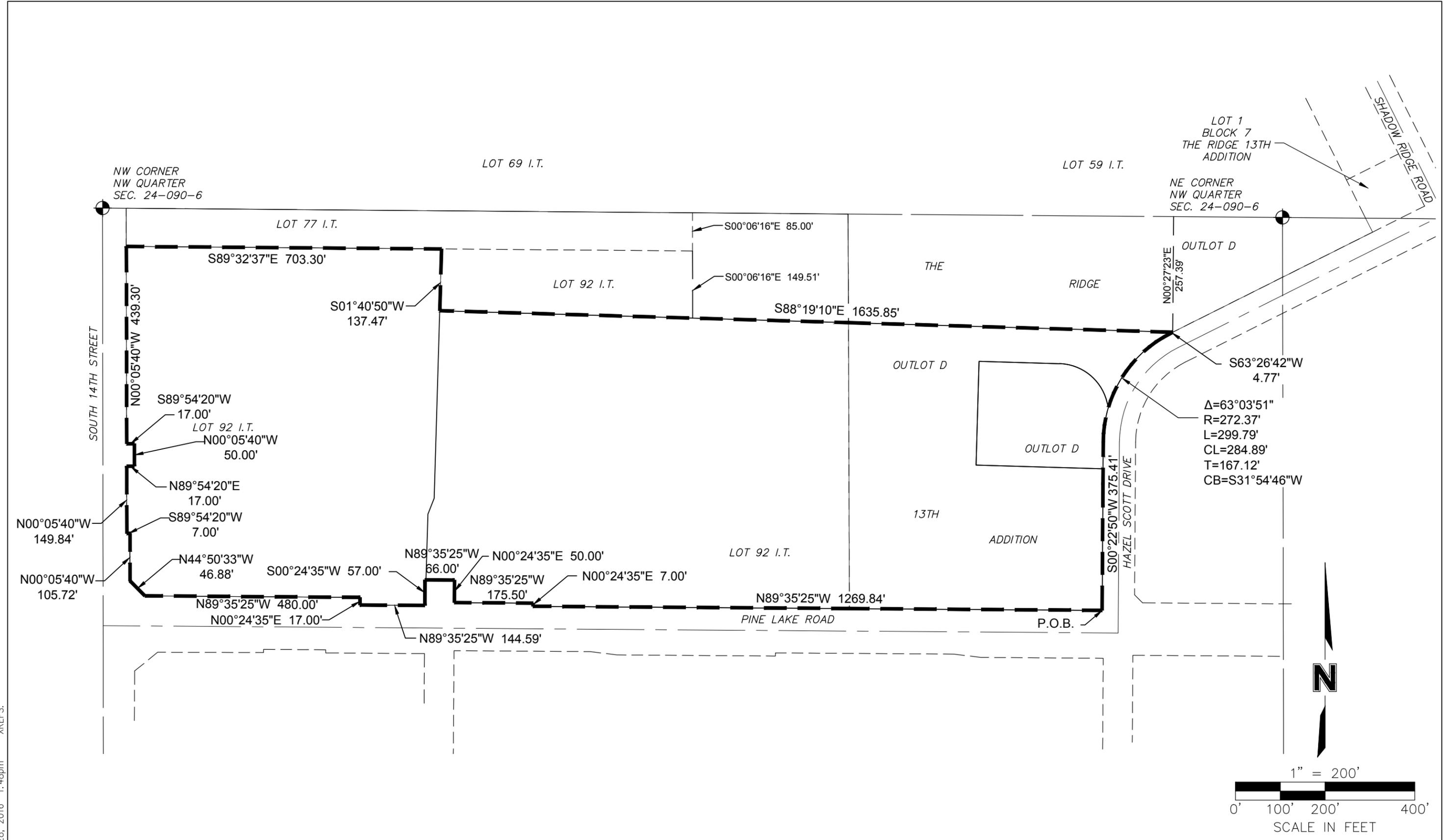
- LANDSCAPE REQUIREMENTS**
- SECTION 7.1: 1 TREE PER 6000 SF OF PAVED PARKING AREA AND 3' TALL SCREEN, 60% OF PARKING LOT LENGTH, OF WHICH 50-75% IS CONIFEROUS**
- TREE REQUIREMENT:**
- 1 TREE / 6000 SF OF PAVED PARKING AREA (INC. DRIVE AISLES)
 - PAVED PARKING AREA: 502,105 SF
 - TREES REQUIRED: 84
 - TREES PROVIDED: 111 DECIDUOUS SHADE TREES AND 6 ORNAMENTAL/UNDERSTORY TREES IN PARKING LOT
- SCREENING REQUIREMENTS:**
- SOUTH PARKING LOT SCREENING:** 3 FOOT TALL SCREEN, 60% OF LENGTH, 50-75% CONIFEROUS
 - LENGTH OF SOUTH PARKING LOT: 1106 FEET
 - 60% OF 1106 = 664 FEET
 - REQUIRED: 664 FEET OF 3' TALL SCREEN
 - PROVIDED: 879 FEET (79%); OF 4' TALL SCREEN, 61% IS CONIFEROUS
 - EAST PARKING LOT SCREENING:** 3 FOOT TALL SCREEN, 60% OF LENGTH, 50-75% CONIFEROUS
 - LENGTH OF SOUTH PARKING LOT: 238 FEET
 - 60% OF 238 = 143 FEET
 - REQUIRED: 143 FEET OF 3' TALL SCREEN
 - PROVIDED: 188 FEET (79%); OF 4' TALL SCREEN, 60% IS CONIFEROUS
- SECTION 7.5: 10' TALL SCREEN, 60% OF PARKING LENGTH, OF WHICH 50-75% IS CONIFEROUS**
- SCREENING REQUIREMENTS:**
- NORTH BOUNDARY SCREENING:**
 - LENGTH OF SOUTH PARKING LOT: 1617 FEET
 - 60% OF 1617 = 970 FEET
 - REQUIRED: 970 FEET OF 10' TALL SCREEN
 - PROVIDED: 1232 FEET (76%); OF 10' TALL SCREEN, 76% IS CONIFEROUS
- SECTION 7.6: MIN. 3 DECIDUOUS SHADE TREES AND 400 SF OF SHRUB/ORNAMENTAL GRASS PER 10,000 SF BUILDING COVERAGE**
- BUILDING COVERAGE:** 156,508 SF
- REQUIRED:** 47 DECIDUOUS SHADE TREES AND 6260 SF SHRUBS/ORN. GRASSES
- PROVIDED:** 111 DECIDUOUS SHADE TREES AND 22,134 SF OF SHRUBS/ORNAMENTAL GRASSES IN PARKING LOT ISLANDS
- SECTION 7.11: 10' TALL SCREEN**
- PROVIDED:** 10' TALL CONTINUOUS SCREEN OF 20 CONIFEROUS EVERGREENS



COSTCO
WHOLESALE
LINCOLN, NE

LINCOLN, NE
SMITHGROUP JJR
201 DEPOT STREET
ANN ARBOR, MI 48104
734.662.4457
www.smithgroupjir.com

DWG: F:\Projects\015-3421\40-Design\Survey\SRVY\Xref\V_SRVY_ReZoning_015-3421.dwg
 DATE: Sep 28, 2016 1:48pm
 USER: sfryc



PROJECT NO: 015-3421
 DRAWN BY: DAT
 DATE: 09.28.16

CEMETERY DISTANCE EXHIBIT

	601 P. Street, Suite 200 P.O. Box 84608 Lincoln, NE 68508 TEL 402.474.6311 FAX 402.474.5160	EXHIBIT
	(Empty space for notes or additional information)	

Mr. Steve Henrichsen
City of Lincoln
Planning Department
555 S. 10th Street, Suite 213
Lincoln, NE 68508

RE: Costco Wholesale Corporation
Change of Zone Application; Comprehensive Plan Amendment
and Use Permit Application

Dear Steve:

Please accept this letter on behalf of my client Costco Wholesale Corporation as well as the current owner The Lincoln Memorial Park Cemetery Association in their pursuit to obtain a Change of Zone for approximately 40.58 acres located at the intersection of S 14th Street and Pine Lake Road as depicted on the attached site plan. They are seeking to rezone and replat 6 parcels from AG and R-1 to R-1 and B-2.

Besides the Change of Zone request, we have been informed that this project also requires a Comprehensive Plan Amendment. In addition, Costco Wholesale Corporation as the contract purchaser is seeking a Use Permit for the construction of a member's only retail warehouse along with accessory uses to include an attached Tire Center and a free-standing Fueling Facility. The proposed location for this development will be constructed on approximately 21.73 acres on the north side of Pine Lake Road between the intersections of S. 16th Street and Hazel Scott Drive. The proposal also includes a 1.74 acre lot to be sold to a future user as well as a 1.10 acre on site detention pond. Within this application we are requesting on behalf of the seller Use Permit approval for three lots (1.35 acres, 1.35 acres and 1.18 acres) west of the proposed Costco parcel adjacent to Pine Lake Road and immediately south of the existing funeral home. We acknowledge that any site plan specific use permits for the proposed lots other than Costco's designated lot must be addressed with the City at the time a user has been determined.

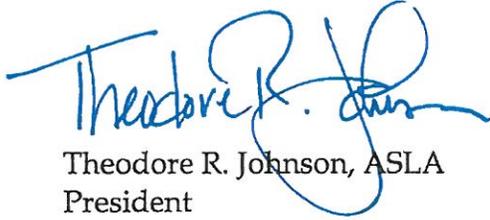
We also request a waiver for this project as it applies to Design Standards for Screening and Landscaping, Section 7.5. Section 7.5 requires screening between the funeral home and the cemetery to the north due to the new commercial zoning requested abutting the residential zoning of the cemetery. We have been informed this isn't necessary as they are essentially the same premises. We do not believe we are in need of any other

additional waivers and/or variances; however wish to request that should any arise during the review process, that we will be able to incorporate such requests at that time.

If you have any questions, and/or require any additional information, please do not hesitate to contact our office at 630-368-0840.

Sincerely,

TJ Design Strategies, Ltd.

A handwritten signature in blue ink, appearing to read "Theodore R. Johnson". The signature is fluid and cursive, with a large loop at the end.

Theodore R. Johnson, ASLA
President
Authorized Costco Wholesale Representative

Current Project - Agency Review Report

Agency Name	User Name	Review Cycle	Review Status	Comments	Assignment
Building & Safety	Terry Kathe	1	Did Not Complete		Individual
		2	In Review		Individual
City Recycling Office	Gene Hanlon	1	Did Not Complete		Individual
County Health	chris schroeder	1	Recommend Approval	<p>During the construction process, the land owner(s) will be responsible for controlling off-site dust emissions in accordance with Lincoln-Lancaster County Air Pollution Regulations and Standards Article 2 Section 32. Dust control measures shall include, but not limited to application of water to roads, driveways, parking lots on site, site frontage and any adjacent business or residential frontage. Planting and maintenance of ground cover will also be incorporated as necessary.</p> <p>Developers are responsible for all mosquito control issues during the building process and all outlots, green-spaces, and/or natural corridors subsequently controlled by the owner, tenant, occupant, lessee, or otherwise, for that subdivision would be responsible for vectors of zoonotic disease in those areas.</p>	Individual
		2	Recommend Approval	see review 1 comments.	Individual
Development Review Manager	steve henrichsen	1	Corrections Required		Individual
		2	Corrections Required		Individual
Emergency Communications	Kelly Davila	1	Did Not Complete		Individual
Fire Department	patrick borer	1	Recommend Approval	Lincoln Fire and Rescue recommends approval of this application.	Individual
Historic Districts/Capitol Environs	ed zimmer	1	Corrections Required	<p>The concept elevations provided are of lesser material interest than the sample photographs.</p> <p>As in the "Sian Wall" photo.</p>	Individual

Current Project - Agency Review Report

				<p>1) Provide curbs between pedestrian areas and parking or driving aisles. 2) At least half of the required masonry area shall be brick.</p> <p>General Note 23 on Sheet 1 is confusing and appears difficult to implement. Revise General Note 23 on Sheet 1 to read: 23. All buildings in the Center, including the Costco building, shall meet the following design standards: A.) All buildings will include at least 20% of the gross area of vertical surfaces as decorative CMU, brick or stone. Half of those required masonry materials shall be brick or stone, the remainder and any masonry areas in excess of 20% may be decorative CMU. The required masonry materials shall be used to the extent feasible on facades oriented towards public streets, with any remaining required masonry used on other facades. (B.) All buildings in the center will utilize building materials and colors similar to the Costco building. (C.) Calculations of the gross area of vertical surfaces, the required area of masonry materials, and the proposed area of masonry on each façade shall be provided at the time of application for building permits.</p>	
		2	Pending		Individual
LES	les reviews	1	Corrections Required	<p>9/29/2016 See required easements on DWG. Relocation will be needed for facilities in R.O.W, Hazel Scott Dr. & 14th & Pine Lake. DG & SLS</p>	First In Group
Lincoln Police Department	Lincoln Police Department	1	Did Not Complete		First In Group
Lower Platte South NRD	kyle hauschild	1	Recommend Approval	ok KH	Individual
Parks & Recreation	mark canney	1	Recommend Approval		First In Group
		2	Pending		Individual
Planning Dept	brenda thomas	1	No Review Required		First In Group
Public Works -	bob simmering	1	Corrections Required	9-30	First In Group

Current Project - Agency Review Report

Engineering Services				See Burkland letter regarding access, ROW dedication and traffic issues. EO required for all public improvements including turn lanes, traffic signals, trail construction.	
		2	Recommend Approval	See 9-30 comment	Individual
Public Works - Watershed Management	tim	1	Corrections Required	<ul style="list-style-type: none"> - Both "East Offsite" and "Middle Offsite" are not accurate drainage areas. Please revise the drainage areas to reflect the actually grading. As shown now, there will be areas from both "East Offsite" and "Middle Offsite" that do not get to the inlets, but sheet flow into the "East" area, increasing the flow in this area. - Need to see actual grading for the "West" detention cell. Cannot show it as "Future". - Need to show the detail for the outlet structure in both detention cells, making sure to include the WQCV outlet. Also show a detail of the state storage elevations for the 2, 10, and 100-year in both detention cells. - In the drainage report, it is shown that the outfall for the "East Pond" is a 15" RCP, with a stair-stepped weir. Yet, on the site plan, the outfall is shown as a 21" RCP that taps into the existing 42" RCP. This existing 42" RCP then taps into the 48" RCP down Hazel Scott Dr. There are two issues here. The first issue is that the outfall shown in the report is different than what is shown on the site plan. Please correct either the report or the site plan. The second issue is how the 21" RCP connects into the 42" RCP. Please show a detail of this. - Detention cell side slopes need to be 4:1. Currently they look closer to 3:1. Also, the top width of the detention cell needs to be 14 ft. Currently it appears there is no top width. - Set a Minimum Floor Elevation for Lot 6 at 1 ft above the 100-year elevation in the adjacent detention cell. - Add a note that states that once the "East Offsite" area is developed, detention will be required for that area so that the post development flows are not greater than the pre development flows. - Please submit the official WQCV form. The provided calculations look ok, but the official form needs to be submitted. Also include a maintenance plan for the WQ 	Individual

Current Project - Agency Review Report

				requirements. A maintenance agreement is not needed at this time, but will be required prior to Building Permit.	
		2	Corrections Required	<p>My first comment from the previous review has still not been addressed. This grading plan does not accurately represent the drainage areas, and is proposing grading outside the limits of the Use Permit. This means that the drainage report is also inaccurate because it is based on inaccurate drainage areas. This plan cannot be approved at this time because it is inaccurate. Furthermore, with the requirement to dedicate ROW at the southeast corner of the property for the potential roundabout at Hazel Scott Dr., the hesitancy to show the exact location of the West Detention Cell, and the inaccurate drainage areas, it appears there is not enough area on this property to meet the detention requirements. The following is a list of issues that need to be addressed before this plan can be approved:</p> <ol style="list-style-type: none"> 1. Revise the grading plan to show accurate drainage areas to follow the contours, not the property lines. Then revise the drainage report, using the accurate drainage areas. 2. Show how the 100-year is routed from the East Offsite and West Offsite areas. Any 100-year overland flow has to be in a flowage easement. 3. Existing East area leaves the site at the southeast end of the property through an existing 42" pipe. Proposed East Offsite area leaves the site at a different area. Please show how this change in outlet points will be handled. Does the existing storm sewer have the capacity to handle this new outlet point? 4. We need to see the exact location of the West Detention Cell. It is not acceptable to say the location will be determined at the time of development, and this detention cell should not be called "future". Also, this detention cell needs to either be on an outlot (similar to Outlot A) or be in an easement. 5. I mentioned this in my previous review – all detention cell side slopes need to be at a 4:1. Also, Parks and Recreation will need to review the location of the detention cell in relation to the bike trail to determine if it meets their clearance requirements. Typically we require a 10 ft buffer on all sides of a detention cell. 6. Plot out the 100-year ponding elevation for both 	Individual

Current Project - Agency Review Report

				<p>detention cells, making sure there is at least 1 ft of freeboard on all sides of the detention cells.</p> <p>7. The East Detention Cell needs to be reconfigured so that it does not conflict with the required ROW dedication for the roundabout at Hazen Scott Dr. The new location of the East detention cell closer to the existing roadway has also created possible conflicts with sightlines for vehicles on Hazel Scott Dr. with bicycle traffic along Old Cheney. Traffic will need to review this to verify if the location of the detention cell is acceptable.</p> <p>8. Revise Note 21 to say that the WQ maintenance agreement is the only thing that is not needed at this time, but will be required prior to Building Permit.</p> <p>9. Need to add a railing to the East Detention Pond outlet structure.</p>	
Public Works & Utilities - Wastewater	brian kramer	1	Recommend Approval	All lots appear to have sanitary sewer.	Individual
Public Works & Utilities - Water	dave beyersdorf	1	Recommend Approval		First In Group
Stronger Safer Neighborhoods	Jon Carlson	1	Did Not Complete		Individual
United States Post Office	kerry kowalski	1	Recommend Approval	Recommend approval on the condition all new delivery addresses are established in Centralized Box Units (CBUs) which will be purchased and installed at the developer's expense in a location mutually agreed upon by the developer and the US Postal Service.	First In Group
Windstream	Jeff Zoller	1	Did Not Complete		Individual